

DEFINITIVE SITE PLAN

FOR

114 AUSTIN STREET

WORCESTER, MASSACHUSETTS 01609

RECORD APPLICANT:
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MA 01532

RECORD OWNER:
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MA 01532

LAND PLANNERS—CIVIL ENGINEERS:
 J.M. GRENIER ASSOCIATES INC.
 118 TURNPIKE ROAD SUITE 200
 SOUTHBOROUGH, MA 01772
 (508) 845-2500

LAND SURVEYORS:
 GEO/NETWORK LAND SURVEY INC.
 645 CHANDLER STREET SUITE 7
 WORCESTER, MA 01610

ZONING DISTRICT: RESIDENCE, GENERAL (RG-5)
 OVERLAY DISTRICT: CROWN HILL LHD

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2/5/24

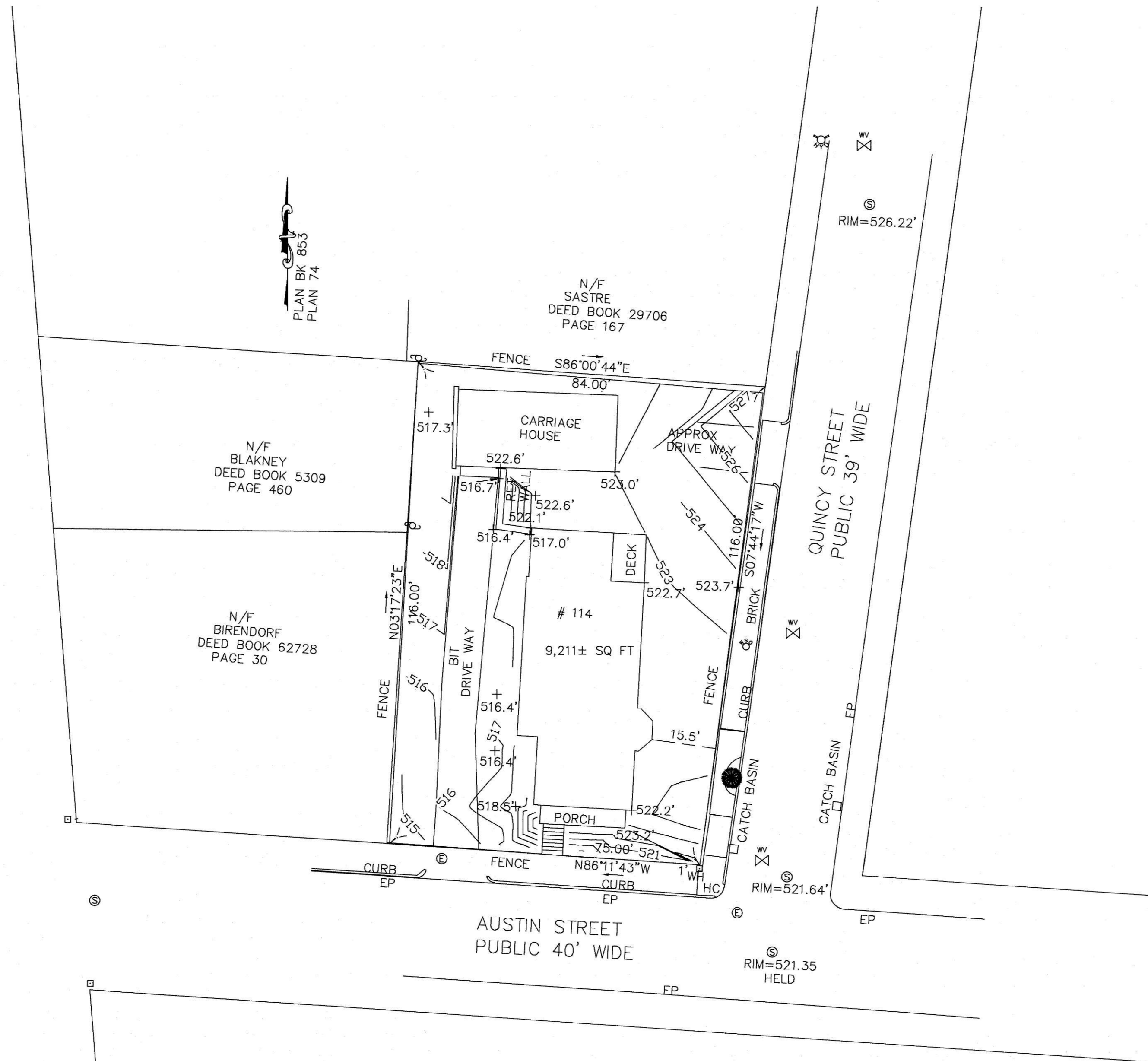
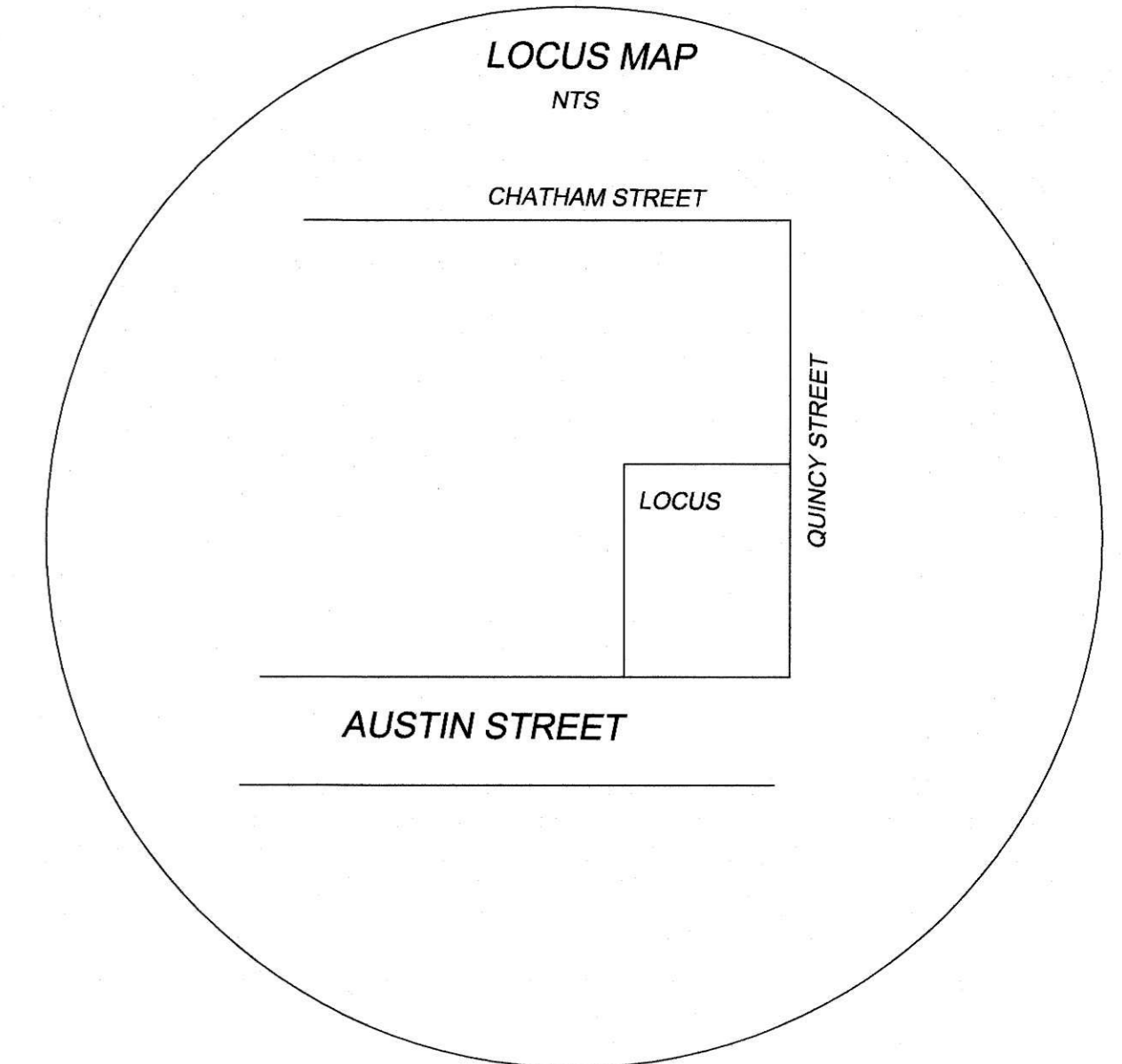
REV. NO.	DATE	REVISION
SCALE: AS SHOWN		DATE: FEBRUARY 5, 2024
COVER SHEET		
SHEET NO.: SHEET 1 OF 9		PROJECT NO.: G-684

PLAN REFERENCES

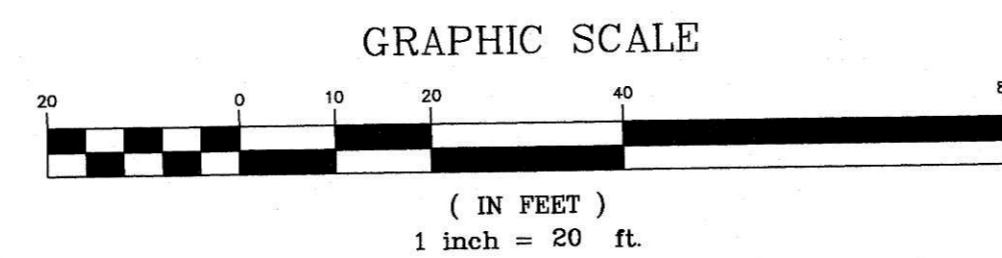
PLAN BOOK 412 PLAN 100
 PLAN BOOK 859 PLAN 99
 PLAN BOOK 853 PLAN 74

CITY OF WORCESTER PLANS
 NEWBURY STREET (H-32 6898)
 AUSTIN STREET (H-9435-1)
 QUINCY STREET (H-11 6986)

FOR REGISTRY OF DEEDS USE ONLY



- NOTES**
- 1.) DATUM TAKEN FROM UTILITY PLAN FROM CITY OF WORCESTER
 CONTRACTOR TO CONFIRM DATUM
 - 2.) CONSTRUCTION ON LOTS OR LAND IS SUBJECT TO ANY EASEMENTS, RIGHT OF WAYS, RESTRICTION RESERVATIONS OR LIMITATIONS ON RECORD
 - 3.) UTILITIES SHOWN ARE FROM FIELD LOCATIONS IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER AND THE CONTRACTOR TO VERIFY THE SIZE, ELEVATION AND LOCATION, AND TO CONTACT "DIG-SAFE ST LEAST 72 HOURS PRIOR TO ANY EXCAVATION, DEMOLITION OR CONSTRUCTION



MGL C41 S81X SURVEYOR'S CERTIFICATE:

I HEREBY CERTIFY THAT THE PROPERTY LINES SHOWN ON THIS PLAN ARE THE LINES DIVIDING EXISTING OWNERSHIPS, AND THE LINES OF THE STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW LINES FOR DIVISION OF EXISTING OWNERSHIP OR FOR NEW WAYS ARE SHOWN.

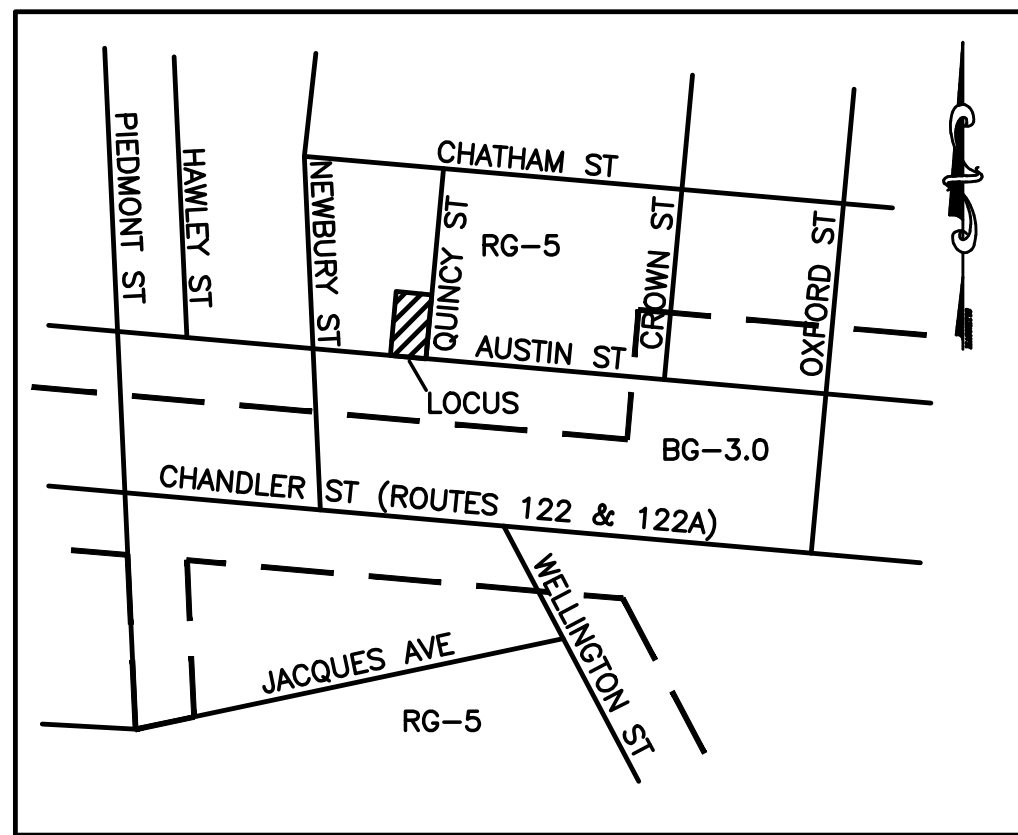
2/1/29
 DATE SURVEYOR
 Gary L. Baugher



PLAN OF LAND
 114 AUSTIN STREET
 WORCESTER, MA

PREPARED FOR: DANIEL AND REBECCA YARNIE
 FEBRUARY 1, 2024 SCALE 1" = 20'

GEO / NETWORK LAND SURVEY, INC.
 645 CHANDLER STREET SUITE 7
 WORCESTER, MASSACHUSETTS 01610
 508-755-7003 FAX 508-755-8003



LOCUS: N.T.S.

ZONING SUMMARY

CURRENT ZONE: RESIDENCE, GENERAL (RG-5)
 OVERLAY ZONE: CROWN HILL LHD

	REQUIRED MFD LOW RISE	REQUIRED SINGLE FAMILY ATTACHED	EXISTING MFD LOW RISE	PROPOSED SINGLE FAMILY ATTACHED
MIN. LOT AREA	8,000 SQ.FT.(1)	11,000 SQ.FT.(2)	9,211 SQ.FT.	9,211 SQ.FT.**
LOT REGULARITY FACTOR(3)	0.4	0.4	0.964	0.964
MIN. FRONTAGE	65'	100'	116.00'	116.00'
MIN. FRONT YARD SETBACK	15'	15'	15.5'	8.5**
MIN. EXTERIOR SIDE YARD SETBACK	10'	10'	7.3*	2.0**
MIN. SIDE YARD SETBACK	10'	8'	37.5'	4.6**
MIN. REAR YARD SETBACK	15'	15'	29.1'	9.0**
MIN. ACCESSORY SETBACK	5'	5'	4.6*	--
MAX. FRONT YARD IMPERVIOUS	50%	50%	5.1%	20.0%
MAX. BUILDING COVERAGE	--	--	29.0%	38.0%
MAX. BUILDING HEIGHT	50' (3+ STORIES)	35' (2+ STORIES)	3 STORIES	34'3" (2+ STORIES)

(1) 5,000 SQ.FT./FIRST D.U. + (1,000 SQ.FT./D.U.)*3 = 8,000 SQ.FT.
 (2) 2,200 SQ.FT./D.U.*5 = 11,000 SQ.FT.
 (3) $R=16*A/P^2$, WHERE A=AREA AND P=PERIMETER

* PRE-EXISTING NONCONFORMING
 ** VARIANCE GRANTED FROM ZONING BOARD OF APPEALS

PARKING SPACE REQUIREMENTS

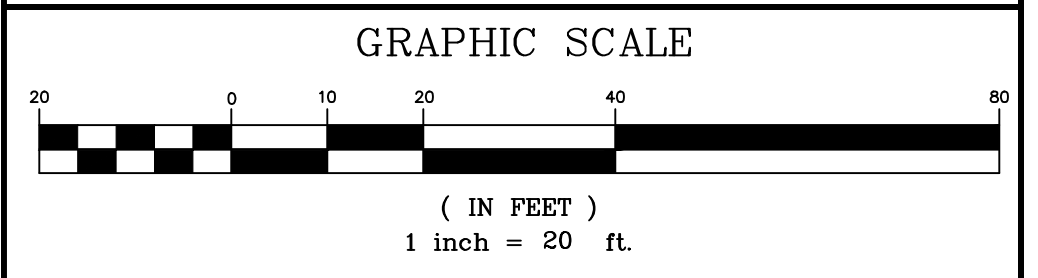
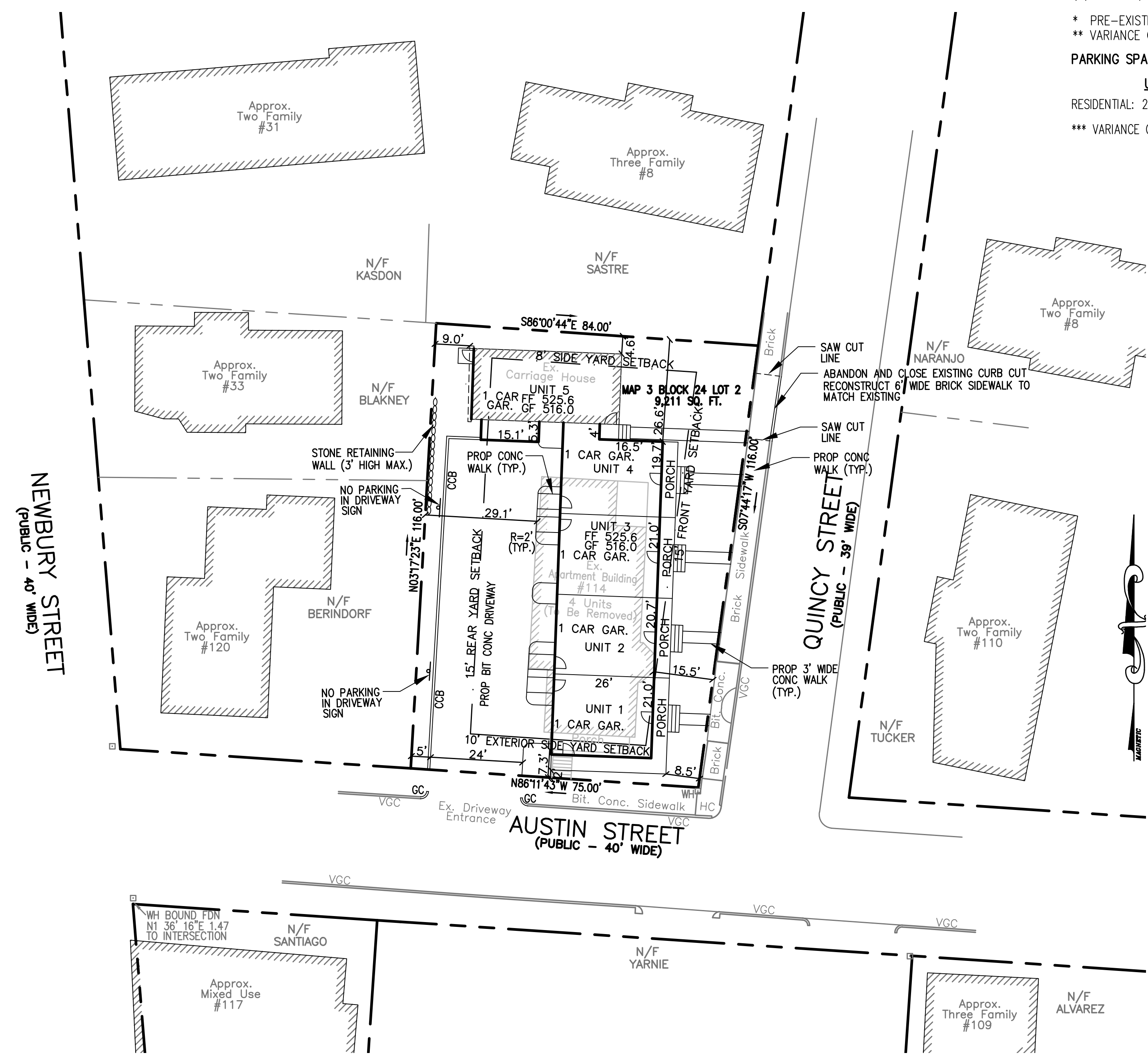
USE	REQUIRED	PROPOSED
RESIDENTIAL: 2/D.U X 5 UNITS =	10 SPACES	5 SPACES***

*** VARIANCE GRANTED FROM ZONING BOARD OF APPEALS

LEGEND:

---	EXISTING PROPERTY LINE
—520—	EXISTING CONTOUR - HIGH
—518—	EXISTING CONTOUR - LOW
—520—	PROPOSED CONTOUR - HIGH
—518—	PROPOSED CONTOUR - LOW
=====	EXISTING EDGE PAVEMENT
=====	EXISTING CURB
=====	PROPOSED EDGE OF PAVEMENT
=====	PROPOSED CURB
=====	EXISTING DRAIN LINE
=====	PROPOSED DRAIN LINE
=====	EXISTING WATER LINE
=====	PROPOSED WATER LINE
=====	EXISTING SEWER LINE
=====	PROPOSED SEWER LINE
=====	PROPOSED EROSION CONTROL

NOTE:
 WRTA ROUTES 6 & 7 ARE LOCATED WITHIN WALKING DISTANCE TO THE SUBJECT PROPERTY (AT CHANDLER/PEIDMONT, CHANDLER WELINGTON, CHANDLER/QUEEN).



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 FOR
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 WORCESTER, MASSACHUSETTS 01609

PREPARED FOR:
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MASSACHUSETTS 01532

PROPERTY OWNER:
 POLAR VIEWS, LLC
 89 WEST MAIN STREET UNIT 101
 NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
 J.M. GRENIER ASSOCIATES INC.
 118 TURNPIKE ROAD SUITE 200
 SOUTHBOROUGH, MASSACHUSETTS 01772

TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE: 1" = 20' **DATE:** FEBRUARY 5, 2024

LAYOUT PLAN

SHEET NO.: SHEET 3 OF 9 **PROJECT NO.:** G-684



2/5/24

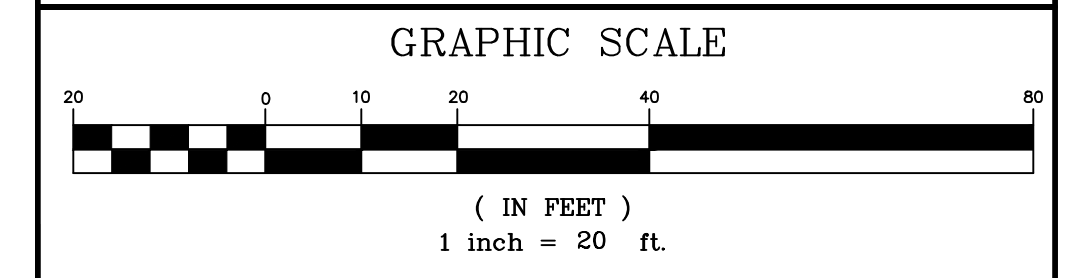
TEST HOLE DATA

TH 1
 ELEV. 517.0
 0-24" FILL
 24-126" C LS
 ESHGW >126" (ELEV. 506.5)

LEGEND:

	EXISTING PROPERTY LINE
	EXISTING CONTOUR - HIGH
	EXISTING CONTOUR - LOW
	PROPOSED CONTOUR - HIGH
	PROPOSED CONTOUR - LOW
	EXISTING EDGE PAVEMENT
	EXISTING CURB
	PROPOSED EDGE OF PAVEMENT
	PROPOSED CURB
	EXISTING DRAIN LINE
	PROPOSED DRAIN LINE
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING SEWER LINE
	PROPOSED SEWER LINE
	PROPOSED EROSION CONTROL

- NOTES:**
- EXISTING CONDITIONS SURVEY BY GEO/NETWORK LAND SURVEY INC. 645 CHANDLER STREET SUITE 7 WORCESTER, MA 01610.
 - REFERENCE CITY OF WORCESTER ASSESSOR'S MAP 3 BLOCK 24 LOT 2.
 - REFERENCE WORCESTER REGISTRY OF DEEDS BOOK 68720 PAGE 1.
 - DATUM IS NAVD88.
 - EXISTING SOILS ON SITE ARE URBAN LAND.



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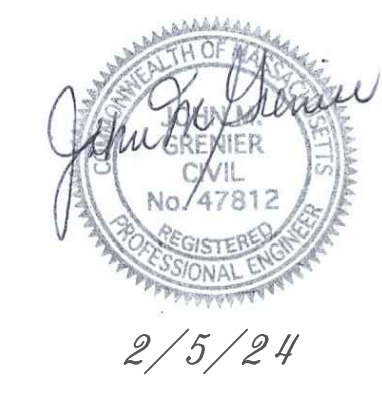
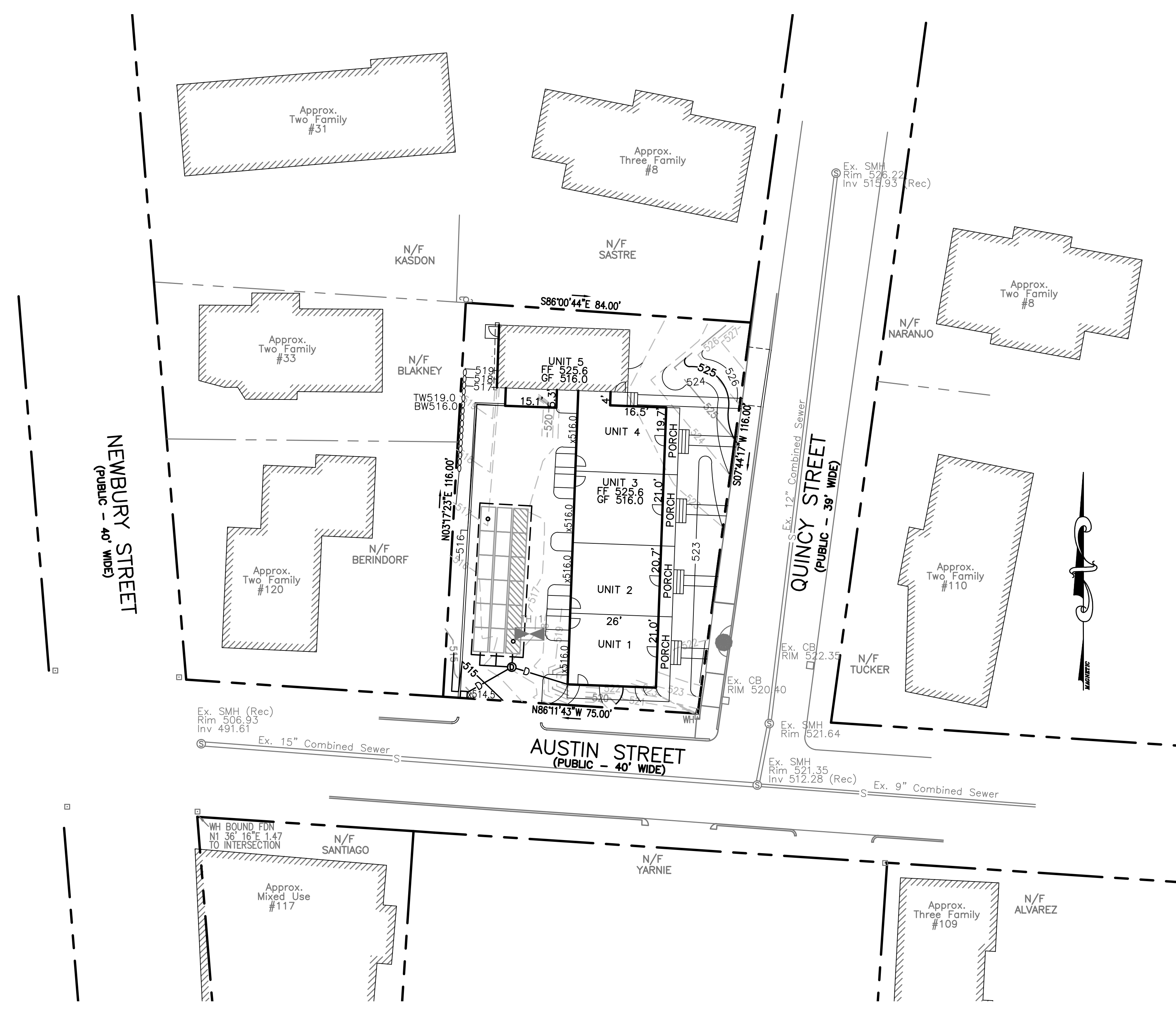
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GRADING AND DRAINAGE PLAN

SHEET NO.: SHEET 4 OF 9 **PROJECT NO.:** G-684

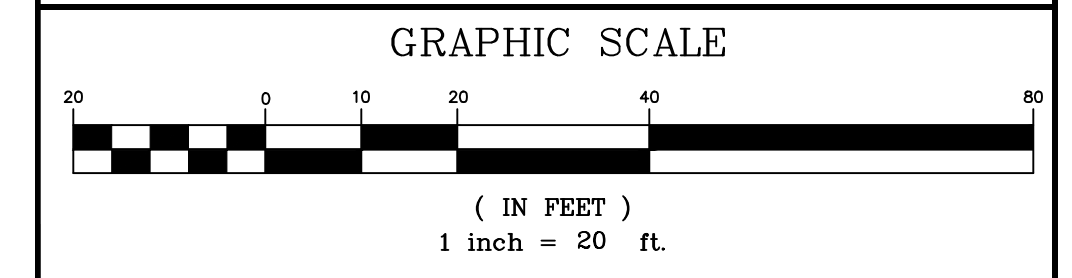


2/5/24

LEGEND:

---	EXISTING PROPERTY LINE
-520-	EXISTING CONTOUR - HIGH
-518-	EXISTING CONTOUR - LOW
-520-	PROPOSED CONTOUR - HIGH
-518-	PROPOSED CONTOUR - LOW
---	EXISTING EDGE PAVEMENT
---	EXISTING CURB
---	PROPOSED EDGE OF PAVEMENT
---	PROPOSED CURB
D	EXISTING DRAIN LINE
D	PROPOSED DRAIN LINE
W	EXISTING WATER LINE
W	PROPOSED WATER LINE
S	EXISTING SEWER LINE
S	PROPOSED SEWER LINE
---	PROPOSED EROSION CONTROL

- NOTES:**
- ALL TRENCHES WITHIN QUINCY STREET SHALL BE BACKFILLED AS REQUIRED BY THE CITY OF WORCESTER.
 - EXISTING CATCH BASINS WITHIN 100' OF THE PROPERTY MUST BE PROTECTED WITH INLET PROTECTION AT ALL TIMES UNTIL THE SITE IS FULLY STABILIZED.
 - SEWER SHALL BE INSTALLED WITH A MINIMUM HORIZONTAL SEPARATION OF 10 FEET FROM ALL WATER SUPPLY LINES. WHEN A 10 FOOT SEPARATION CANNOT BE MAINTAINED THE WATER MAIN SHALL BE INSTALLED IN A SEPARATE TRENCH ABOVE THE SEWER WITH AN 18 INCH VERTICAL SEPARATION BETWEEN THE CROWN OF THE SEWER AND THE INVERT OF THE WATER MAIN. ADEQUATE VERTICAL SEPARATION OF WATER/SEWER CROSSING SHALL BE CONFIRMED.
 - SIZE OF FIRE SERVICE LINE TO BE DETERMINED BY FIRE FLOW REQUIREMENTS
 - DOMESTIC WATER LINE SHALL BE SLEEVED UNDER FOOTING WITH 2" MINIMUM DIAMETER CONDUIT.
 - CONTRACTOR SHALL CONTACT DIG SAFE AT 1-888-344-7233 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION.
 - CONTRACTOR SHALL FIELD VERIFY DIMENSIONS AND CONDITIONS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
 - SHOULD FIELD CONDITIONS VARY FROM THOSE SHOWN, ENGINEER SHALL BE CONTACTED PRIOR TO COMMENCEMENT OF SITE CONSTRUCTION.



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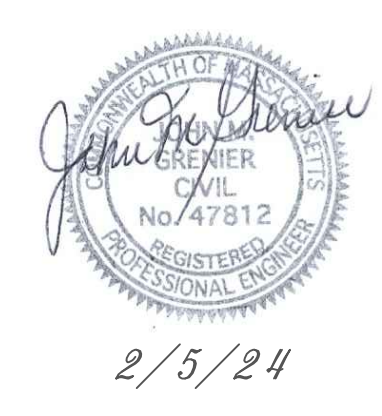
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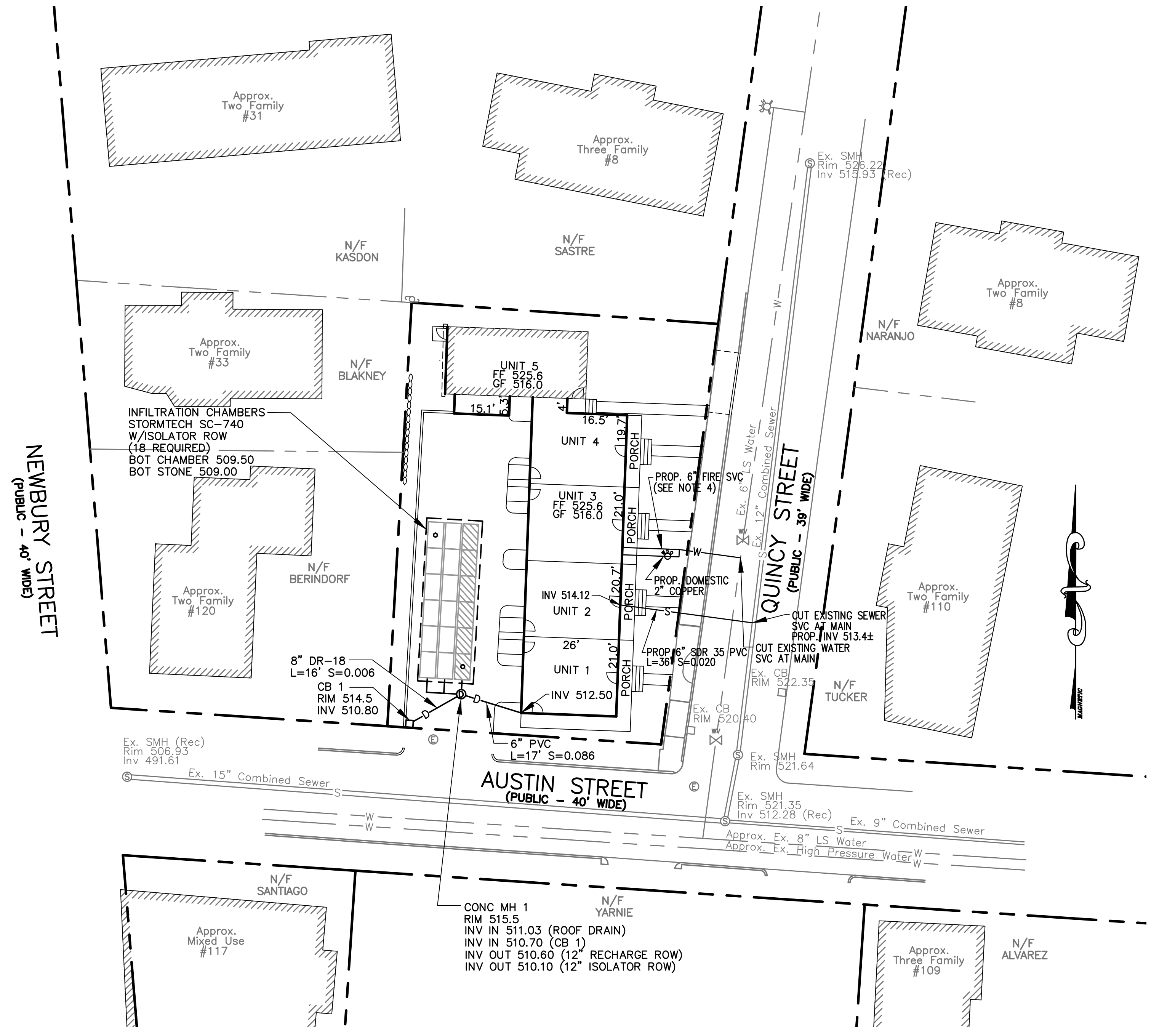
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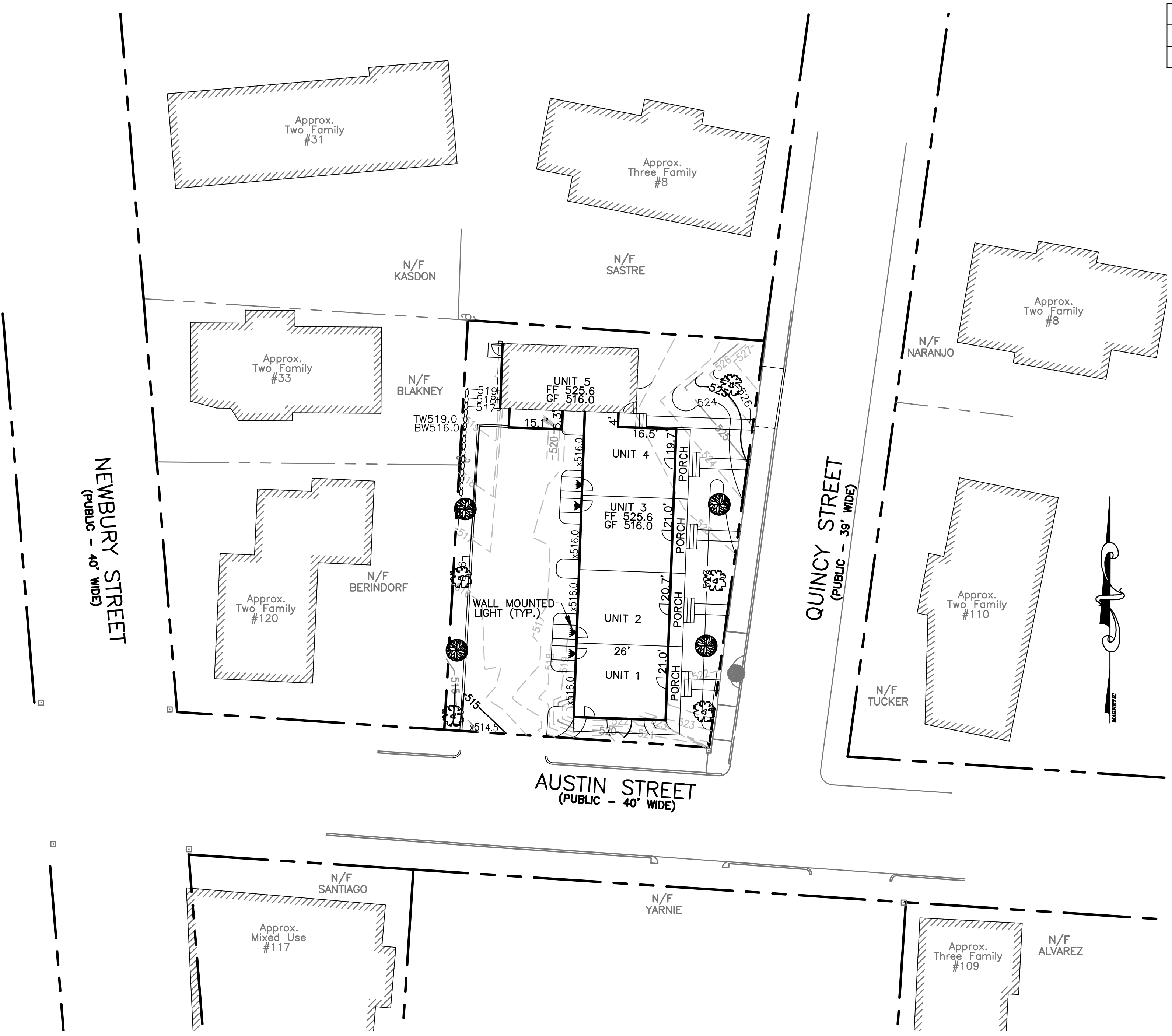
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SHEET NO.: SHEET 5 OF 9	PROJECT NO.: G-684
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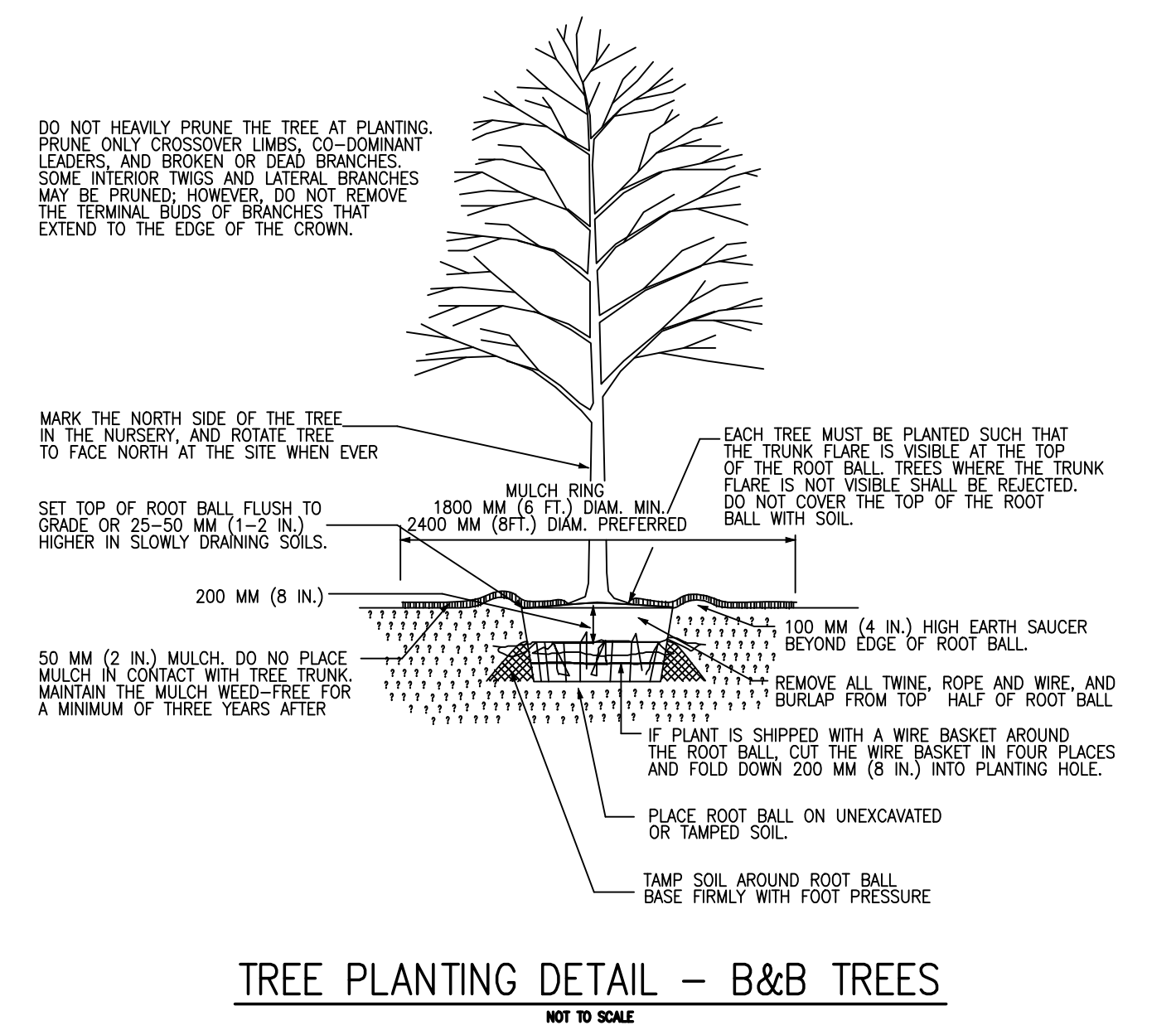
PLANT SCHEDULE

TYPE	KEY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	QTY
TREES		<i>Quercus palustris</i>	Pin Oak	3 1/2" cal.	B&B	5
		<i>Cornus kousa</i>	Kousa Dogwood	3 1/2" cal.	B&B	4

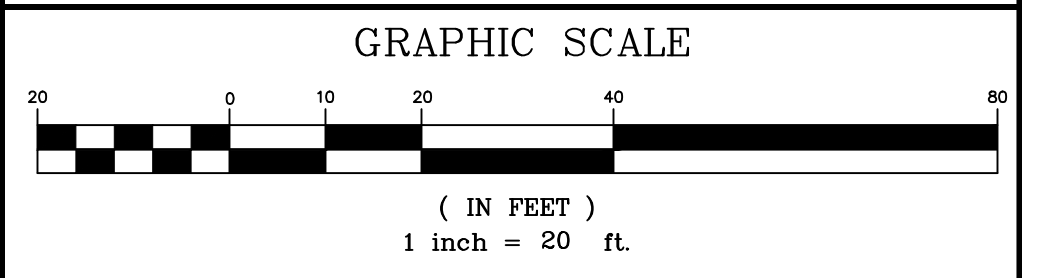
LEGEND:

	EXISTING PROPERTY LINE
	EXISTING CONTOUR - HIGH
	EXISTING CONTOUR - LOW
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	PROPOSED CONTOUR - LOW
	EXISTING EDGE PAVEMENT
	EXISTING CURB
	PROPOSED EDGE OF PAVEMENT
	PROPOSED CURB
	EXISTING DRAIN LINE
	PROPOSED DRAIN LINE
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING SEWER LINE
	PROPOSED SEWER LINE
	PROPOSED EROSION CONTROL

- NOTES:**
- ALL PLANTINGS SHALL BE ASIAN LONGHORNED BEETLE (ALB) RESISTANT.
 - SNOW STORAGE SHALL NOT BE LOCATED WITHIN ANY REQUIRED PARKING SPACES OR LANDSCAPED BUFFERS AND SHALL NOT IMPEDE VISIBILITY. ONCE DESIGNATED SNOW STORAGE AREAS EXCEED CAPACITY SNOW SHALL BE REMOVED FROM THE SITE.
 - LIGHTING SHALL BE WALL MOUNTED AND RESIDENTIAL IN STYLE. ALL LIGHTING SHALL BE DARK SKY COMPLIANT WITH SHIELDING TO PREVENT SPILLOVER AND A MAX TEMPERATURE OF 4000K.



TREE PLANTING DETAIL - B&B TREES
NOT TO SCALE



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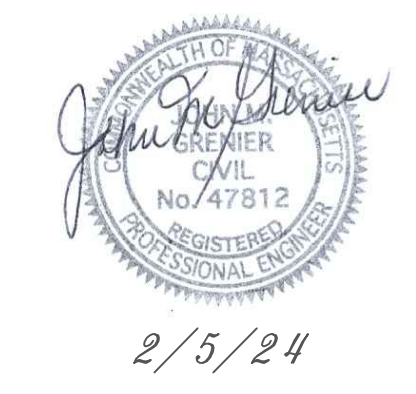
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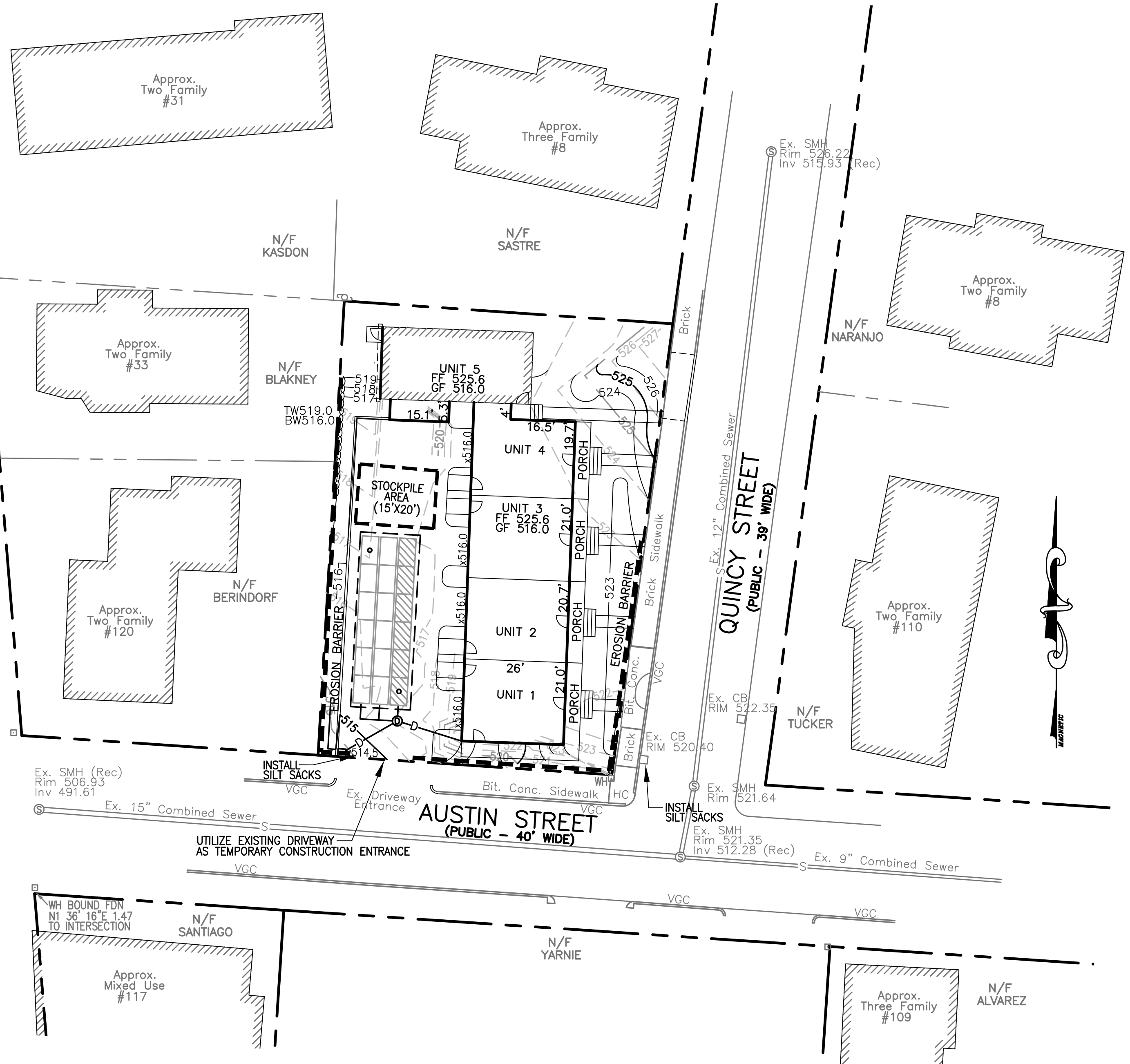
TITLE: LANDSCAPE PLAN

SHEET NO.: SHEET 6 OF 9
PROJECT NO.: G-684



2/5/24

NEWBURY STREET
(Public - 40' Wide)



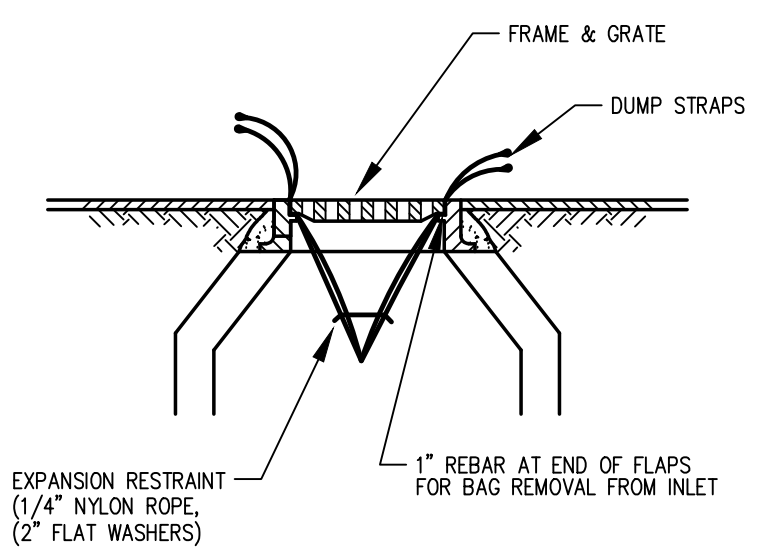
1. Seeding and repairs shall be performed as required. Sediment and debris shall be removed at least once a year, typically in early spring prior to the commencement of the growing season.

2. The catch basin shall be inspected annually. Unit shall be cleaned when accumulated sediments reach a depth of 6 inches. Accumulated sediment must be disposed of in accordance with applicable local state, and federal guidelines and regulations. The contractor will be responsible for the maintenance of the unit until such time as the site work is complete. The maintenance will then be the responsibility of the owner(s).

3. The infiltration chambers shall be inspected after every major storm for the first 3 months to ensure proper stabilization and function. The chambers shall be inspected once per year after that. Water levels should be inspected and recorded for several days after a major storm event to track infiltration capability.

4. The contractor will be responsible for the maintenance of all drainage structures and until such time as the site work is complete. The maintenance will then be the responsibility of the property owners.

STORMWATER COLLECTION & TREATMENT SYSTEM INSPECTION & MAINTENANCE GUIDELINES



CONSTRUCTION SEQUENCING:

- SELECTIVELY CUT TREES AND CLEAR BRUSH TO BE CHIPPED AND HAULED OFF SITE. NOTE THAT THE SITE IS IN THE ASIAN LONGHORNED BEETLE (ALB) REGULATED AREA.
 - DEMOLISH EXISTING APARTMENT BUILDING.
 - STAKE LOCATION OF AND INSTALL EROSION CONTROL BARRIER, CONSTRUCTION ENTRANCE AS DELINEATED ON SITE PLAN.
 - STRIP TOP AND SUBSOIL AS NECESSARY IN WORK AREA. STOCKPILE MATERIAL ON WEST PORTION OF LOT FOR BACKFILLING PURPOSES AT COMPLETION OF FOUNDATION CONSTRUCTION.
 - FORM AND POUR FOUNDATION FOR NEW BUILDING.
 - BACKFILL FOUNDATION AREAS AS NECESSARY
 - CONSTRUCT BUILDING.
 - INSTALL RECHARGE CHAMBERS
 - CONSTRUCT DRIVEWAY AND LOAM AND SEED DISTURBED AREAS.
 - ALL EXCESS MATERIALS TO BE REMOVED FROM THE SITE AS SOON AS PRACTICAL
 - ALL GRADED AREAS TO BE LANDSCAPED, LOAMED AND SEED AS SOON AS PRACTICAL TO REDUCE ANY POTENTIAL EROSION.
- NOTE: PROCESSES AS DESCRIBED ABOVE ARE ESTIMATED TO TAKE 9 MONTHS. WINTER CONDITIONS MAY AFFECT THIS SCHEDULE.

GENERAL:

- THIS PLAN IS INTENDED TO PROVIDE GUIDANCE AND INSTRUCTION TO THE CITY OF WORCESTER AND THE CONTRACTOR(S) IN THE PREVENTION OF EROSION AND SEDIMENTATION OFF SITE. THIS PLAN IS INTENDED TO ALLOW ANY CONDITIONS OF APPROVAL TO BE MORE SPECIFIC IN ADDRESSING ITEMS OF CONCERN. IF CONDITIONS BY THE CITY OF WORCESTER ARE IN CONFLICT WITH REQUIREMENTS AS SPECIFIED ON THE PLANS CONDITIONS AS SPECIFIED BY THE CITY OF WORCESTER SHALL SUPERSEDE THE REQUIREMENTS SPECIFIED ON THESE PLANS.
- PRE-CONSTRUCTION:**
 - AN EROSION CONTROL BARRIER (SEE BELOW) SHALL BE INSTALLED AS DEPICTED ON THE PLANS, BETWEEN THE AREAS TO BE DISTURBED AND RECEIVING DRAINAGE WAY AND STRUCTURES. THIS BARRIER SHALL REMAIN IN PLACE UNTIL ALL TRIBUTARY SURFACES HAVE BEEN FULLY STABILIZED. THE EROSION CONTROL BARRIERS AS SHOWN ON THE PLANS ARE THE MINIMUM PROTECTIVE MEASURES REQUIRED TO PROTECT THE ON AND OFF SITE DRAINAGE SYSTEMS.
 - THE CONTRACTOR SHALL ESTABLISH A STAGING AREA AS SHOWN ON THE PLAN FOR THE OVERNIGHT STORAGE OF EQUIPMENT AND STOCKPILING OF MATERIALS.
 - IN THE STAGING AREA, THE CONTRACTOR SHALL HAVE A STOCKPILE OF MATERIALS REQUIRED TO CONTROL EROSION ON-SITE TO BE USED TO SUPPLEMENT OR REPAIR EROSION CONTROL DEVICES. THESE MATERIALS SHALL INCLUDE, BUT ARE NOT LIMITED TO: HAY BALES, SILT FENCE AND CRUSHED STONE.
 - A TEMPORARY STONE CONSTRUCTION ENTRANCE IS REQUIRED TO PREVENT TRACKING OF SILT, MUD, ETC. ONTO EXISTING ROADS. THE STONE SHALL BE REPLACED REGULARLY AS WELL AS WHEN THE STONE IS SILT LOADED OR EQUIPMENT IS OBSERVED TO BE TRACKING SOIL ONTO THE ROADWAYS. SEE GENERAL CONSTRUCTION REQUIREMENTS, SEE NOTE #4 BELOW.
 - THE CONTRACTOR IS RESPONSIBLE FOR EROSION CONTROL ON & OFF SITE AND SHALL UTILIZE EROSION CONTROL MEASURES WHERE NEEDED, REGARDLESS OF WHETHER THE MEASURES ARE SPECIFIED HEREIN, ON THE PLAN OR IN ANY ORDER OF CONDITIONS.

PRELIMINARY SITE WORK:

- MATERIAL REMOVED SHOULD BE STOCKPILED, SEPARATING THE TOPSOIL FOR FUTURE USE ON THE SITE OR IN A SECURED OFF SITE AREA APPROVED BY THE OWNER. EROSION CONTROLS SHALL BE UTILIZED ALONG THE DOWN SLOPE OF THE PILES IF THE PILES ARE TO REMAIN FOR MORE THAN THREE WEEKS.
- IF INTENSE RAINFALL IS ANTICIPATED, THE INSTALLATION OF SUPPLEMENTAL HAY BALE DIKES, SILT FENCES, OR ARMORED DIKES SHALL BE UTILIZED.
- IF THE SITE CONSTRUCTION OCCURS AT ANY TIME OTHER THAN THE APRIL - DECEMBER CONSTRUCTION SEASON, ALL DRAINAGE SYSTEMS TEMPORARY OR PERMANENT SHALL MAINTAIN TO ENSURE ADEQUATE HYDRAULIC CAPACITY, AND DRAINING CHARACTERISTICS.

EROSION CONTROL MEASURES:

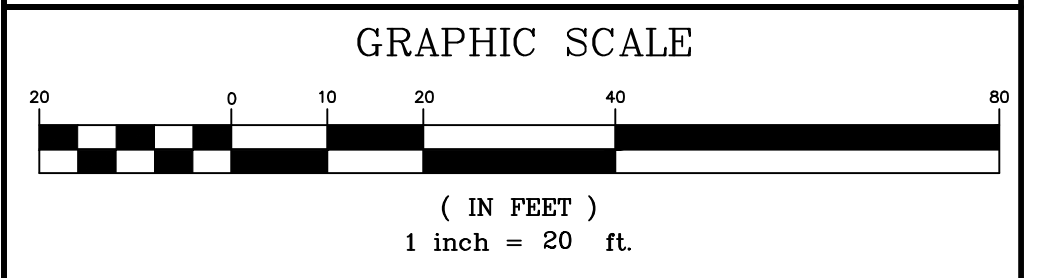
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT EROSION AND SEDIMENTATION ARE CONTROLLED. THIS PLAN SHALL BE ADAPTED TO FIT THE CONTRACTOR'S EQUIPMENT, WEATHER CONDITIONS, AND ANY CONDITIONS ISSUED BY THE CITY OF WORCESTER
- THE MOST IMPORTANT ASPECTS OF CONTROLLING EROSION AND SEDIMENTATION ARE LIMITING THE EXTENT OF DISTURBANCE AND STABILIZING SURFACES AS SOON AS POSSIBLE. OF SECONDARY IMPORTANCE IN EROSION CONTROL IS THE LIMITING THE SIZE AND LENGTH OF THE TRIBUTARY DRAINAGE AREA WITHIN THE WORK SITE AND DRAINAGE STRUCTURES. THESE FUNDAMENTAL PRINCIPLES SHALL BE THE KEY FACTOR IN THE CONTRACTOR'S CONTROL OF EROSION ON THE SITE.
- THE EXISTING SOIL CONDITIONS PROVIDE THE POTENTIAL OF RUNOFF TO OFF-SITE AREAS WITH EROSION POTENTIAL.
- ALL DISTURBED SURFACES SHALL BE STABILIZED A MINIMUM OF 14 DAYS AFTER CONSTRUCTION IN ANY PORTION OF THE SITE HAS CEASED OR IS TEMPORARILY HALTED UNLESS ADDITIONAL CONSTRUCTION IS INTENDED TO BE INITIATED WITHIN 21 DAYS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE AND REPAIR OF ALL EROSION CONTROL DEVICES WITHIN THE LIMIT OF WORK. ALL EROSION CONTROL DEVICES SHALL BE REGULARLY INSPECTED. ANY SEDIMENTS REMOVED FROM THE CONTROL DEVICES SHALL BE DISPOSED OF.
- AT NO TIME SHALL SILT-LADEN WATER BE ALLOWED TO ENTER SENSITIVE AREAS (WETLANDS, OFF-SITE AREAS AND DRAINAGE SYSTEMS). ANY RUNOFF FROM DISTURBED SURFACES SHALL BE DIRECTED THROUGH SETTLING BASINS AND EROSION CONTROL BARRIERS PRIOR TO ENTERING ANY SENSITIVE AREAS.

GENERAL CONSTRUCTION REQUIREMENTS:

- ANY REFUELING OF CONSTRUCTION VEHICLES AND EQUIPMENT SHALL NOT TAKE PLACE INSIDE OF A 100 FOOT BUFFER ZONE, NEAR THE ENTRANCE TO ANY DRAINAGE SYSTEM AND SHALL NOT BE CONDUCTED IN PROXIMITY TO SEDIMENTATION BASINS.
 - NO ON-SITE DISPOSAL OF STUMPS, SOLID WASTE, INCLUDING CONSTRUCTION MATERIALS IS ALLOWED.
 - NO MATERIALS SHALL BE DISPOSED OF INTO THE WETLANDS, OR EXISTING/PROPOSED DRAINAGE SYSTEMS. ALL CONTRACTORS INCLUDING: CONCRETE SUPPLIERS, PAINTERS AND PLASTERERS, SHALL BE INFORMED THAT THE CLEANING OF EQUIPMENT IS PROHIBITED IN AREAS WHERE THE WASH-WATER WILL DRAIN DIRECTLY TO THE SITE DRAINAGE SYSTEMS.
 - THE CONTRACTOR SHALL UNDERTAKE ALL WORK TO LIMIT AIRBORNE SEDIMENTS, ONLY CLEAN, POTABLE WATER MAY BE USED TO CONTROL DUST.
- LANDSCAPING:**
- LANDSCAPING SHALL OCCUR AS SOON AS POSSIBLE TO PROVIDE PERMANENT STABILIZATION OF DISTURBED SURFACES.
 - CONTRACTOR SHALL UTILIZE A VARIETY OF SLOPE STABILIZATION METHODS AND MATERIALS WHICH SHALL BE ADJUSTED TO THE SITE CONDITIONS. EROSION CONTROL BLANKETS OR MIRAFI MIRAMAT (OR SIMILAR PRODUCTS) SHALL BE AVAILABLE ON SITE.
 - IF THE SEASON OR ADVERSE WEATHER CONDITIONS DO NOT ALLOW THE ESTABLISHMENT OF VEGETATION, TEMPORARY MULCHING WITH HAY, TAGKIPED WOOD CHIPS OR OTHER METHODS SHALL BE PROVIDED.
 - ALL DISTURBED SURFACES TO BE PLANTED SHALL RECEIVE A MINIMUM OF 4" TOPSOIL SHALL BE PLACED AND ITS SURFACE SMOOTHED TO THE SPECIFIED GRADES.
 - ALL SLOPES OF 2.5:1 OR GREATER SHALL BE STABILIZED WITH EROSION CONTROL FABRIC.
 - TO ENSURE A DENSE, SUCCESSION GROWTH, SEED IS REQUIRED ON ALL DISTURBED SURFACES.

LEGEND:

- EXISTING PROPERTY LINE
- 520— EXISTING CONTOUR - HIGH
- 518— EXISTING CONTOUR - LOW
- 520— PROPOSED CONTOUR - HIGH
- 518— PROPOSED CONTOUR - LOW
- EXISTING CURB PAVEMENT
- EXISTING CURB
- PROPOSED EDGE OF PAVEMENT
- PROPOSED CURB
- EXISTING DRAIN LINE
- PROPOSED DRAIN LINE
- EXISTING WATER LINE
- PROPOSED WATER LINE
- EXISTING SEWER LINE
- PROPOSED SEWER LINE
- PROPOSED EROSION CONTROL



REV. NO.	DATE	REVISION

TITLE:
DEFINITIVE SITE PLAN
FOR
114 AUSTIN STREET
WORCESTER, MASSACHUSETTS 01609

PREPARED FOR:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

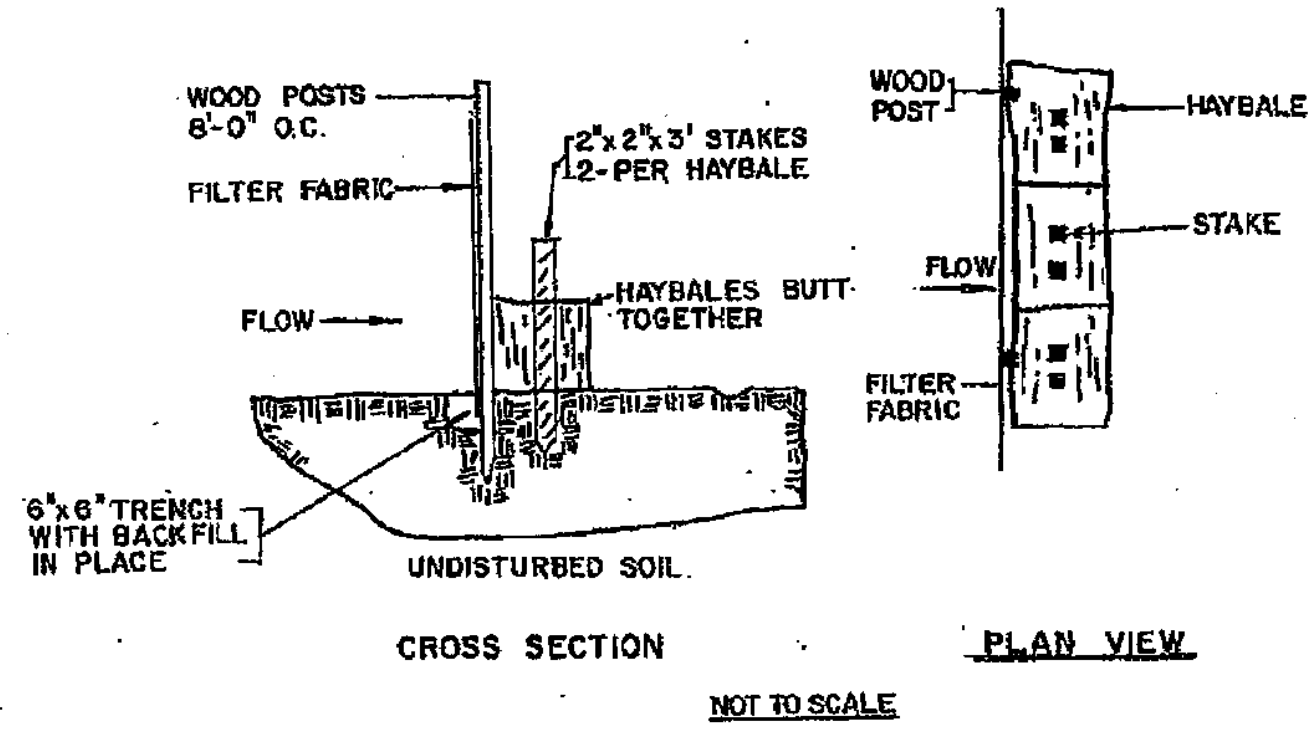
PROPERTY OWNER:
POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532

PREPARED BY:
J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772
TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE: 1" = 20'
DATE: FEBRUARY 5, 2024

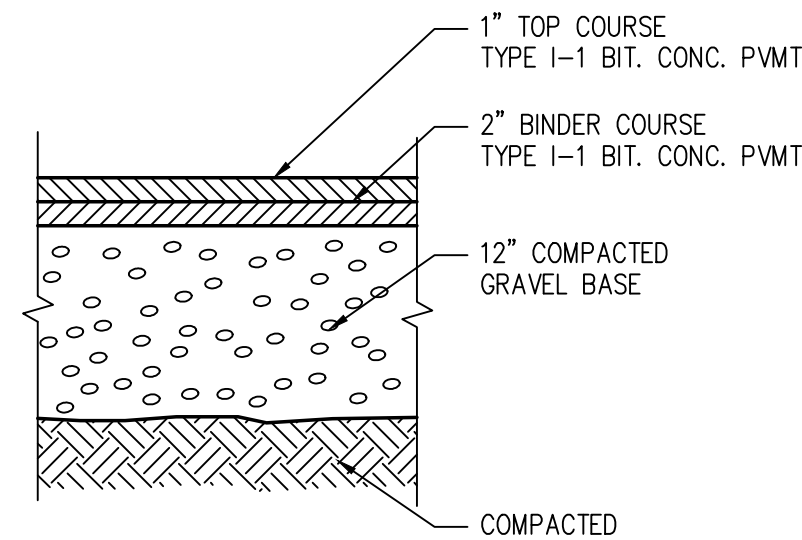
EROSION & SEDIMENTATION CONTROL PLAN

SHEET NO.: SHEET 7 OF 9
PROJECT NO.: G-684

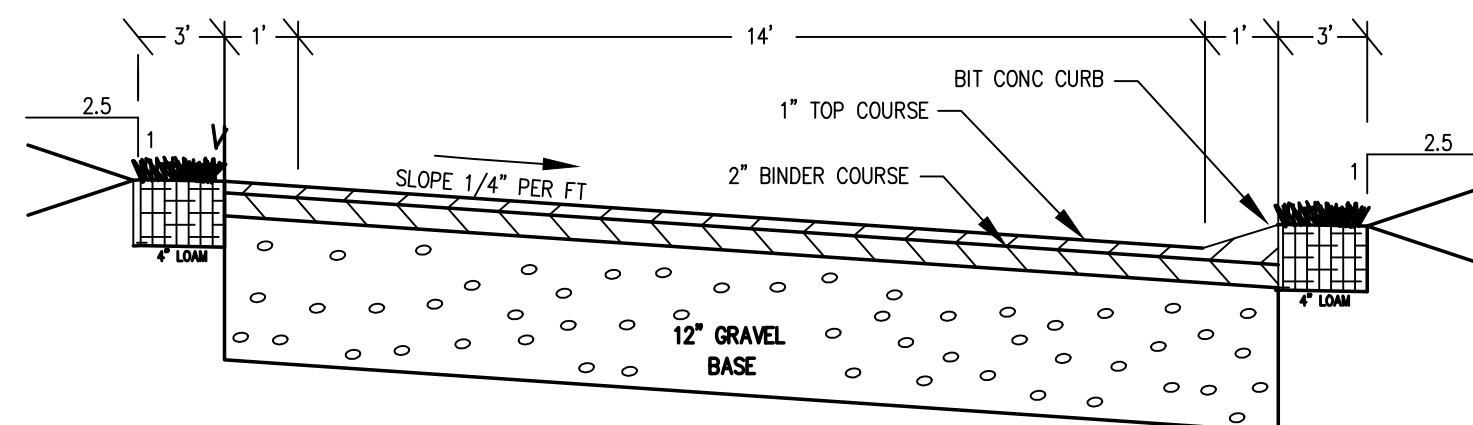


SILTATION CONTROL
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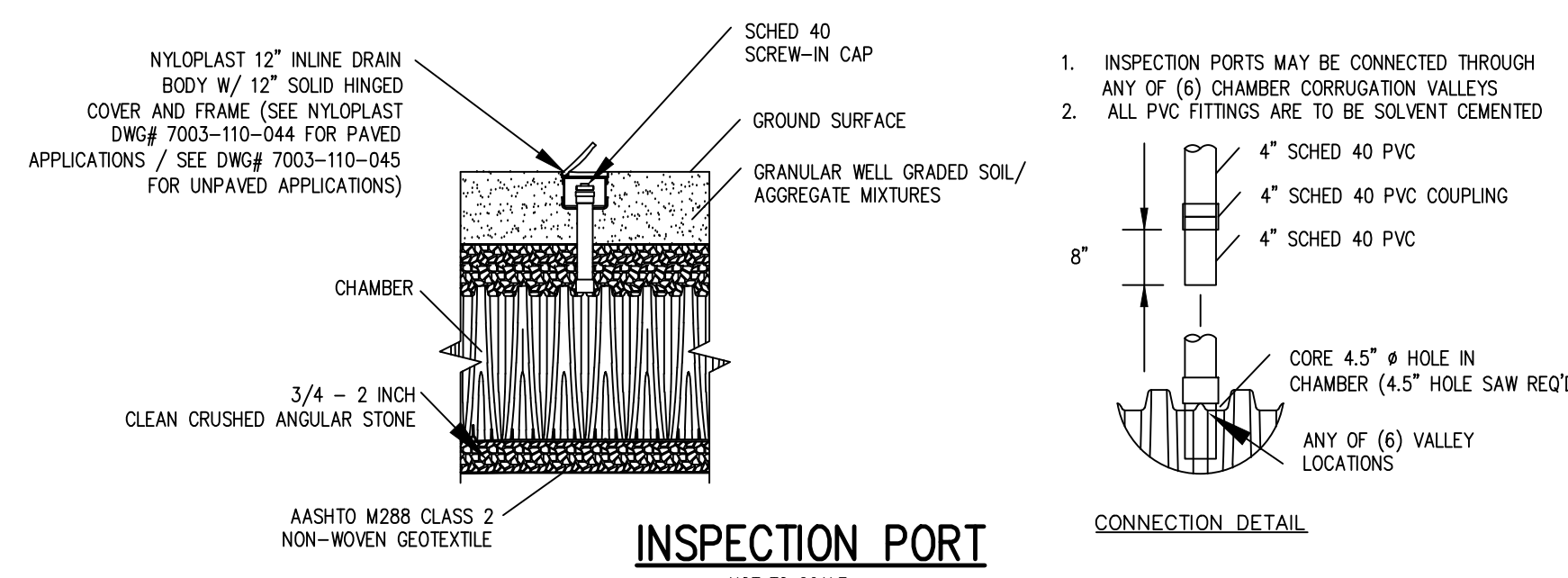
John M. Grenier
J.M. GRENIER
No. 47812
PROFESSIONAL ENGINEER
2/5/24



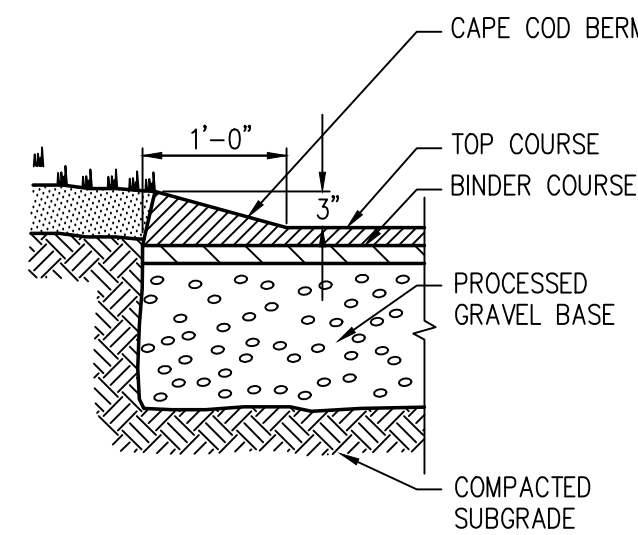
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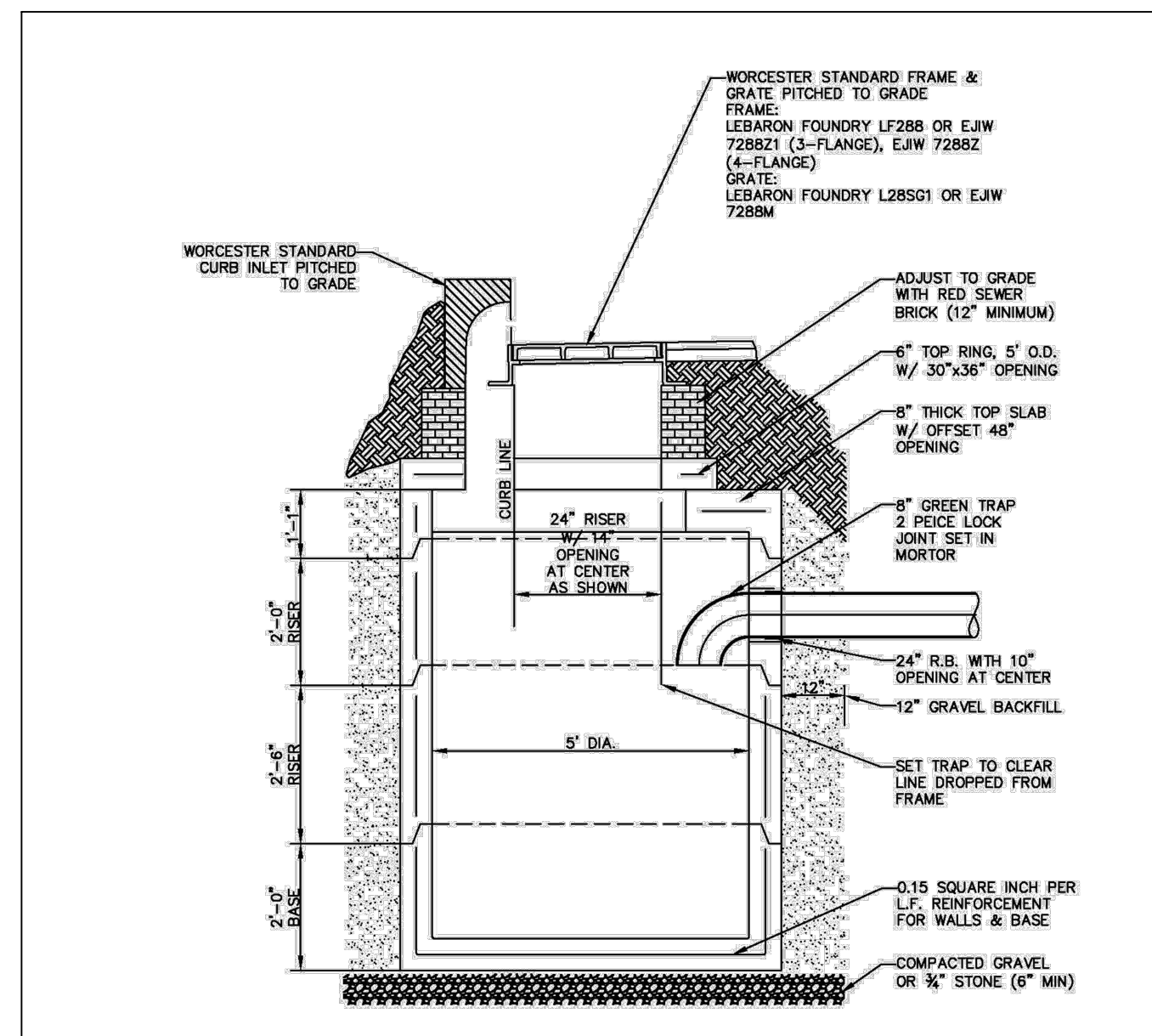
DRIVEWAY CROSS SECTION
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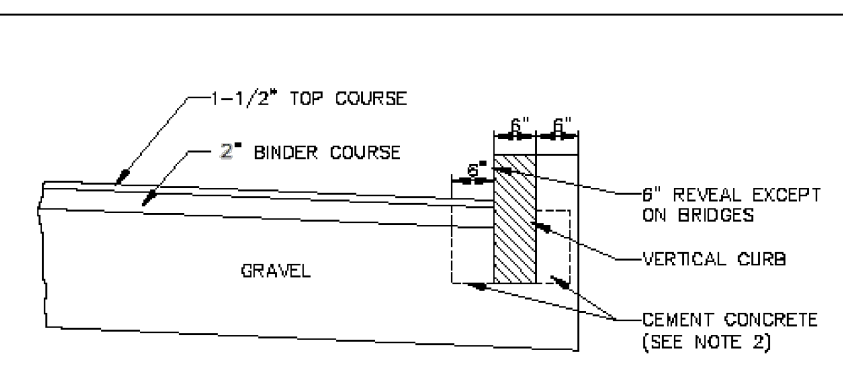
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CAPE COD BERM
NOT TO SCALE



CATCH BASIN DETAIL
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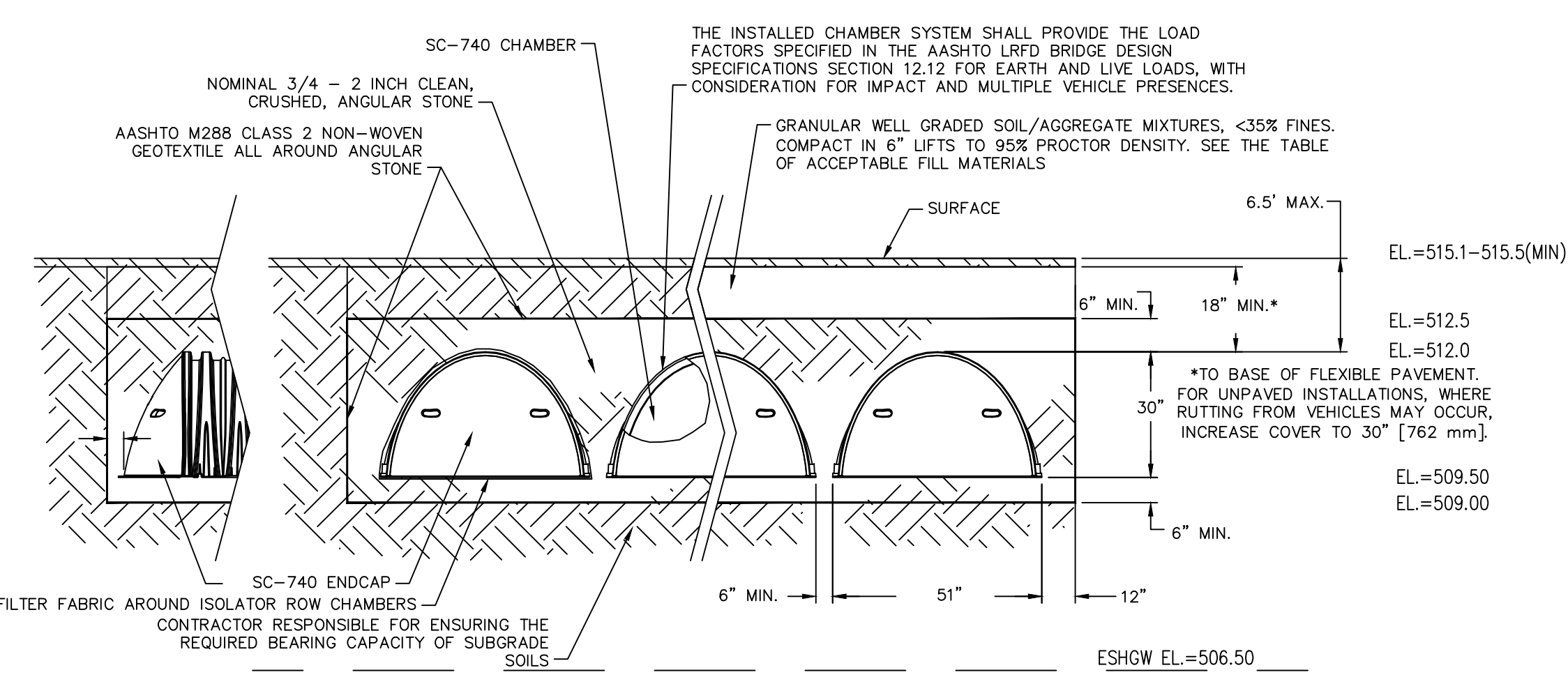


VERTICAL GRANITE CURB DETAIL
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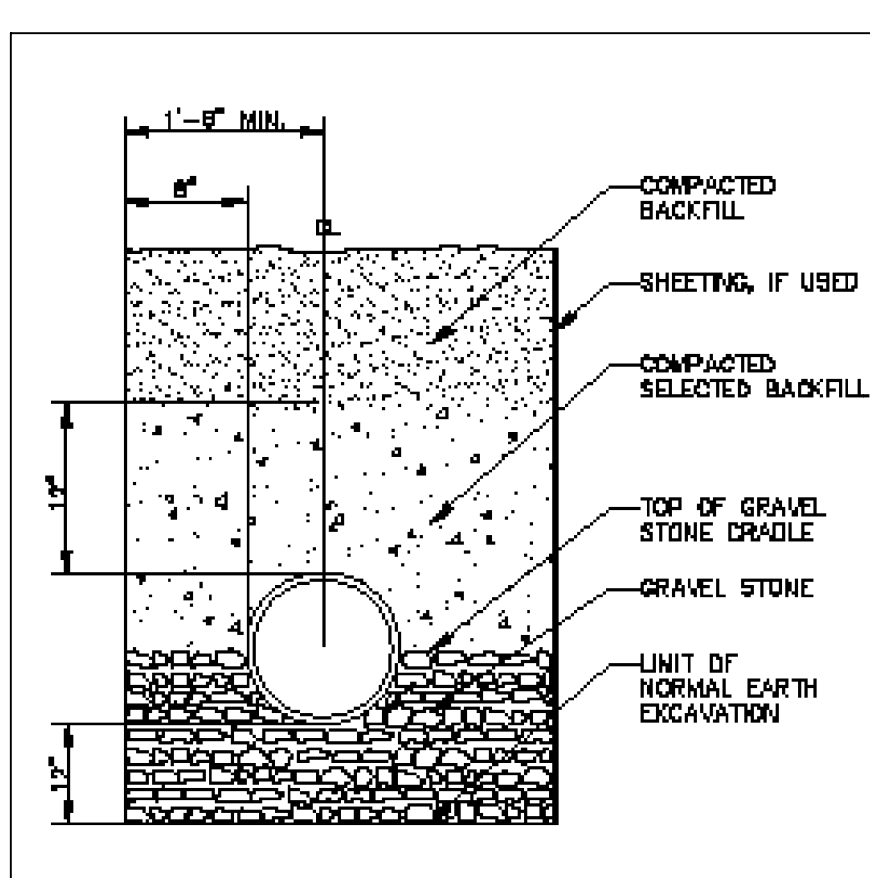
ACCEPTABLE FILL MATERIALS: STORMTECH CHAMBER SYSTEMS

MATERIAL LOCATION	DESCRIPTION	AASHTO M43 DESIGNATION*	COMPACTION/DENSITY REQUIREMENT
(D)	FILL MATERIAL FOR LAYER 'D' STARTS FROM THE TOP OF THE 'C' LAYER TO THE BOTTOM OF FLEXIBLE PAVEMENT OR UNPAVED FINISHED GRADE ABOVE. NOTE THAT PAVEMENT SUBBASE MAY BE PART OF THE 'D' LAYER.	N/A	PREPARE PER ENGINEER'S PLANS. PAVED INSTALLATIONS MAY HAVE STRINGENT MATERIAL AND PREPARATION REQUIREMENTS.
(C)	FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE EMBEDMENT STONE (B LAYER) TO 24" (610 mm) ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBBASE MAY BE A PART OF THIS LAYER.	3, 357, 4, 467, 5, 56, 57, 6, 67, 68, 7, 78, 8, 89, 9, 10	BEGIN COMPACTION AFTER 24" (610 mm) OF MATERIAL OVER THE CHAMBERS IS REACHED. COMPACT ADDITIONAL LAYERS IN 6" (152 mm) LIFTS TO A MIN. 90% STANDARD PROCTOR DENSITY.
(B)	EMBEDMENT STONE SURROUNDING THE CHAMBERS FROM THE FOUNDATION STONE TO THE 'C' LAYER ABOVE.	3, 357, 4, 467, 5, 56, 57	NO COMPACTION REQUIRED.
(A)	FOUNDATION STONE BELOW CHAMBERS FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER.	3, 35, 4, 467, 5, 56, 57	PLATE COMPACT OR ROLL TO ACHIEVE A 95% STANDARD PROCTOR DENSITY†.

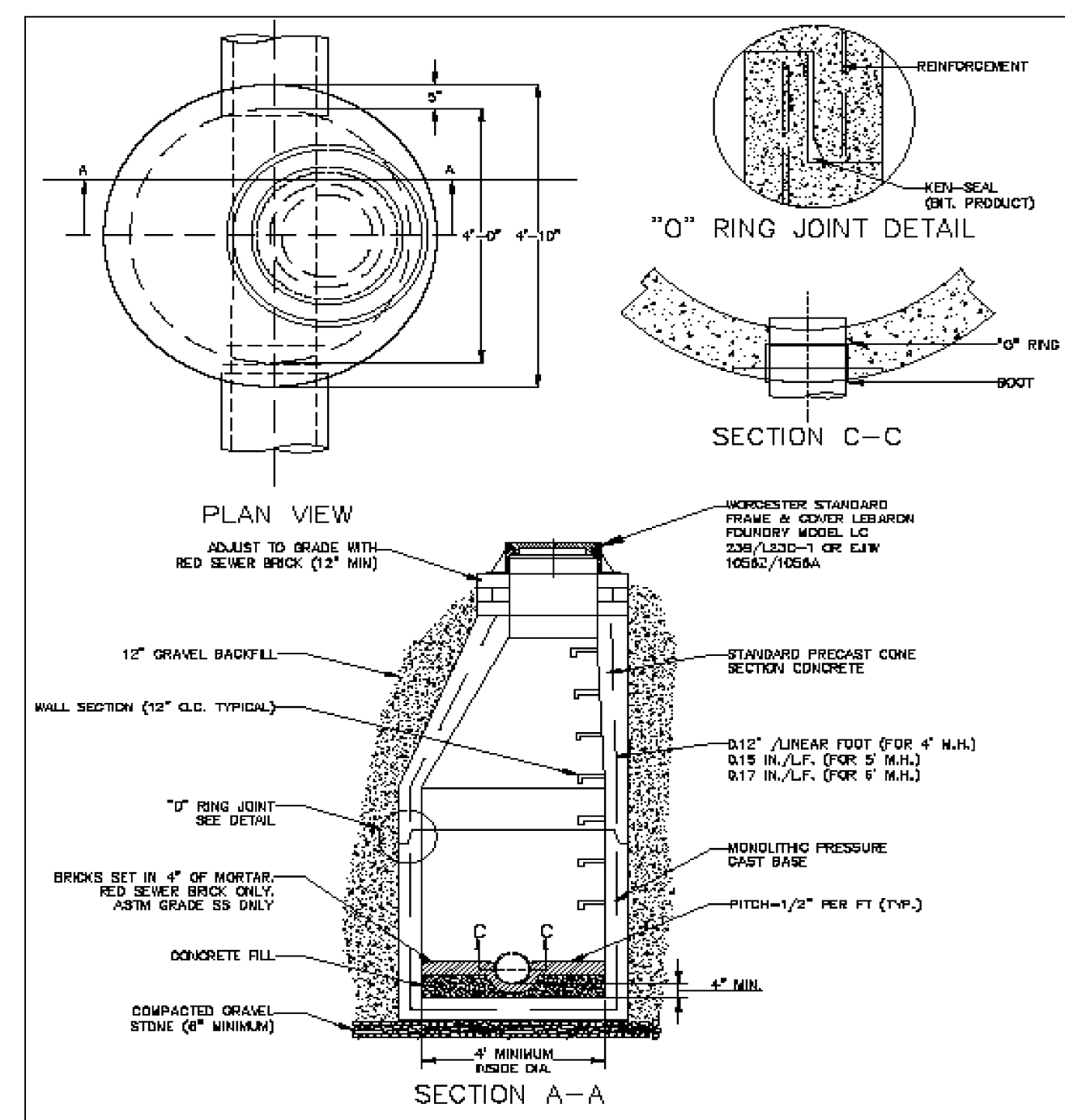
PLEASE NOTE:
1. THE LISTED AASHTO DESIGNATIONS ARE FOR GRADATIONS ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR. FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE: "CLEAN, CRUSHED, ANGULAR NO. 4 (AASHTO M43) STONE".
2. AS AN ALTERNATE TO PROCTOR TESTING AND FIELD DENSITY MEASUREMENTS ON OPEN GRADED STONE, STORMTECH COMPACTION REQUIREMENTS ARE MET FOR 'A' LOCATION MATERIALS WHEN PLACED AND COMPACTED IN 9" (229 mm) (MAX) LIFTS USING TWO FULL COVERAGES WITH AN APPROPRIATE COMPACTOR.



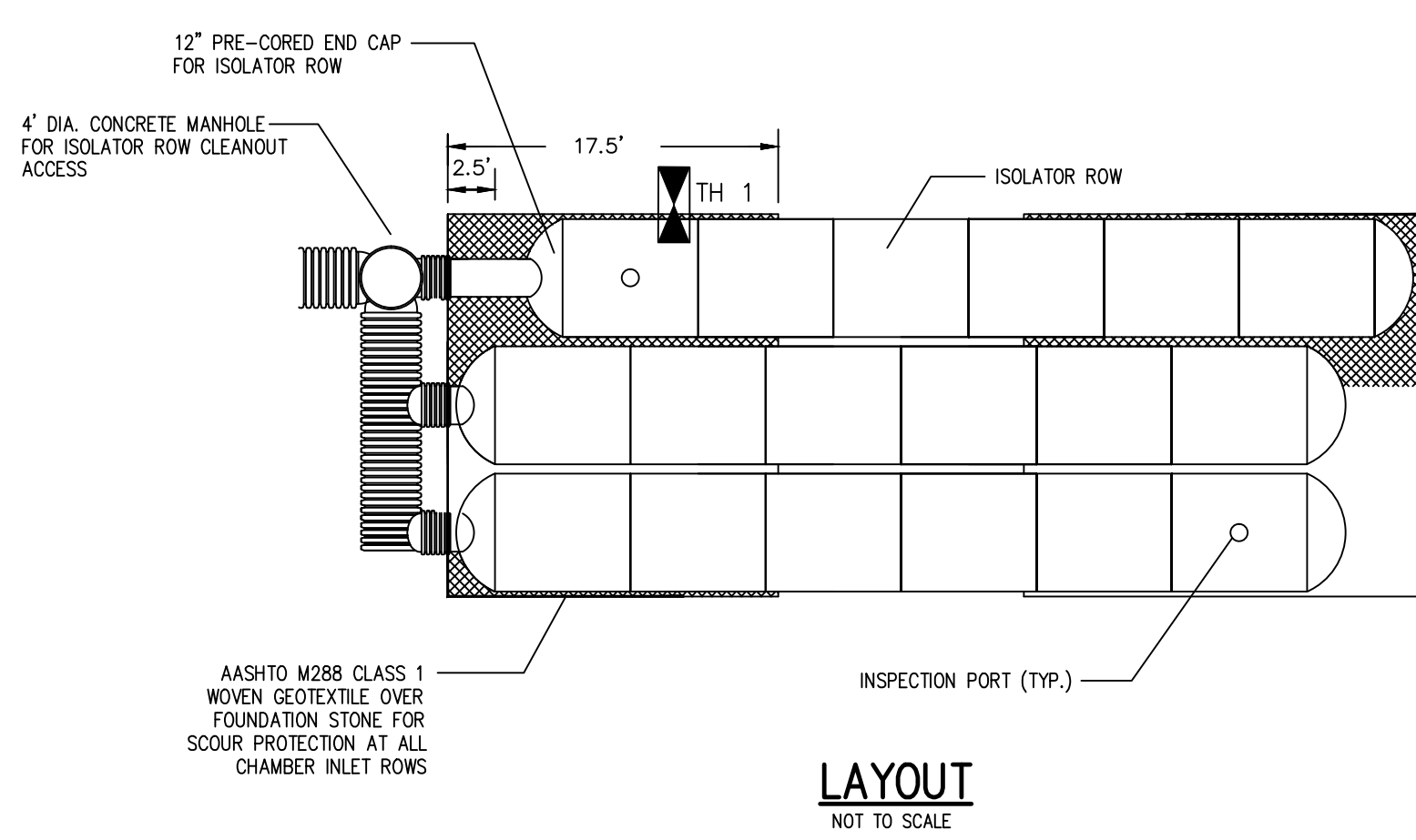
STORMTECH SC-740 INFILTRATION CHAMBERS (OR APPROVED EQUAL) CROSS-SECTION
NOT TO SCALE



DRAIN TRENCH DETAIL
NOT TO SCALE

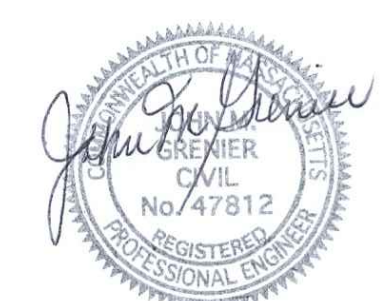


DRAIN MANHOLE DETAIL
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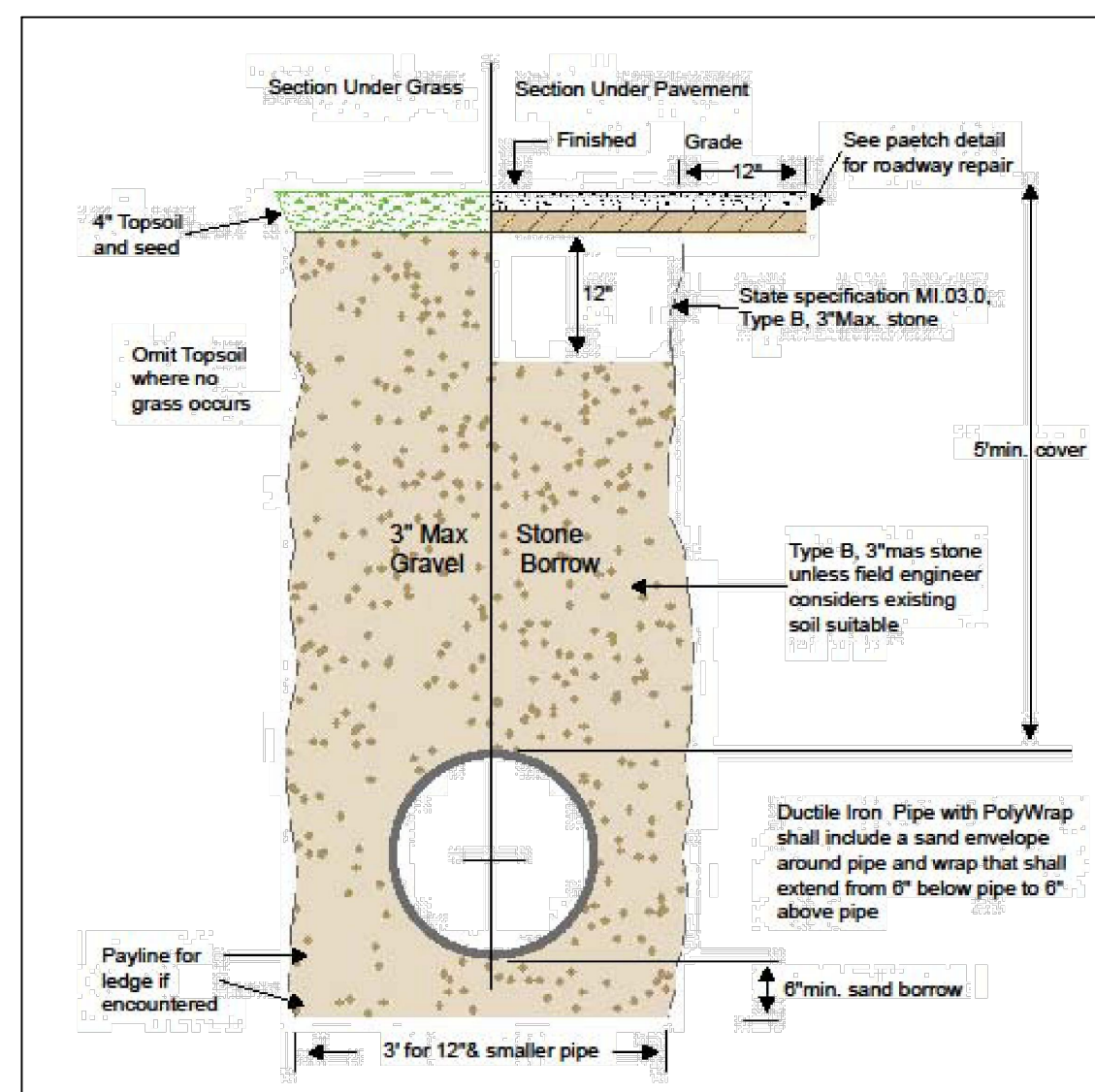
LAYOUT
NOT TO SCALE

- ALL DESIGN SPECIFICATIONS FOR STORMTECH SC-740 CHAMBERS SHALL BE IN ACCORDANCE WITH THE STORMTECH SC-740 DESIGN MANUAL.
- THE INSTALLATION OF STORMTECH SC-740 CHAMBERS SHALL BE IN ACCORDANCE WITH THE LATEST STORMTECH SC-740 INSTALLATION INSTRUCTIONS.
- THE CONTRACTOR IS ADVISED TO REVIEW AND UNDERSTAND THE INSTALLATION INSTRUCTIONS PRIOR TO BEGINNING SYSTEM INSTALLATION. CALL 1-888-892-2694 TO RECEIVE A COPY OF THE LATEST STORMTECH SC-740 INSTALLATION INSTRUCTIONS.
- CHAMBERS SHALL MEET THE DESIGN REQUIREMENTS AND LOAD FACTORS SPECIFIED IN SECTION 12.12 OF THE LATEST EDITION OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.

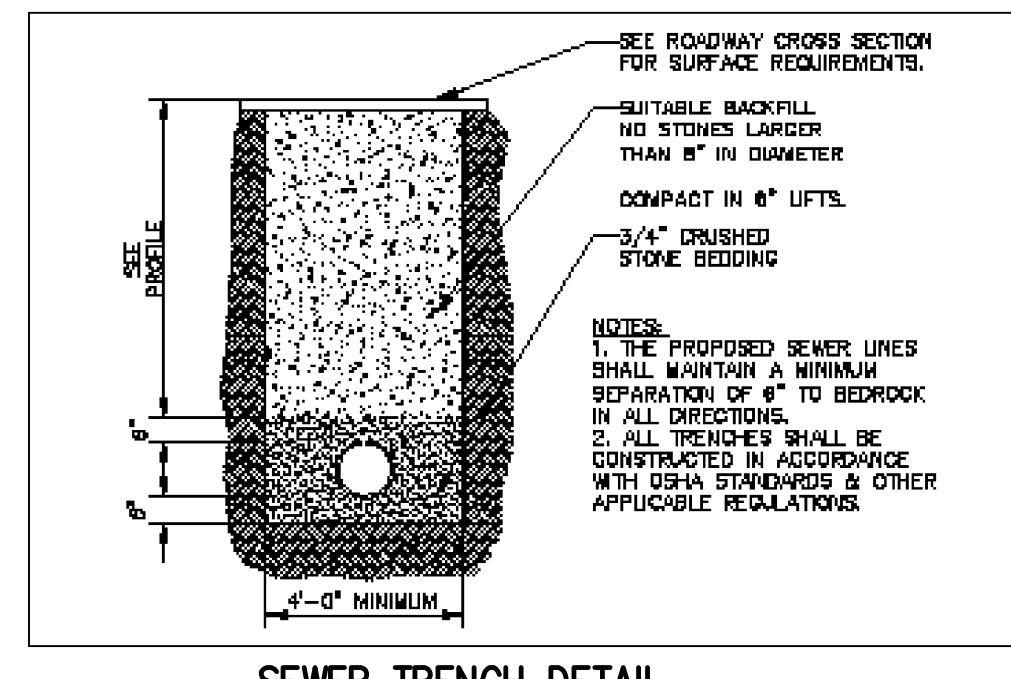


2/5/24

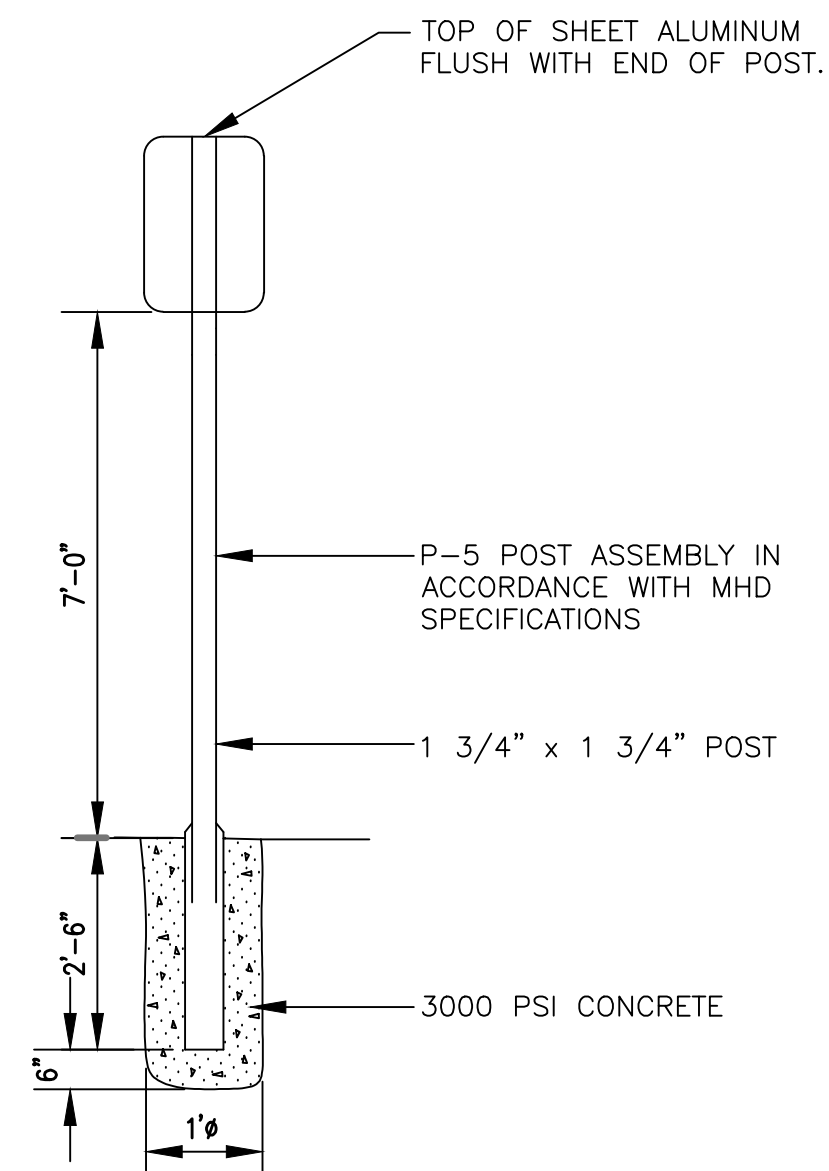
REV. NO.	DATE	REVISION
TITLE: DEFINITIVE SITE PLAN FOR 114 AUSTIN STREET WORCESTER, MASSACHUSETTS 01609		
PREPARED FOR: POLAR VIEWS, LLC 89 WEST MAIN STREET UNIT 101 NORTHBOROUGH, MASSACHUSETTS 01532		
PROPERTY OWNER: POLAR VIEWS, LLC 89 WEST MAIN STREET UNIT 101 NORTHBOROUGH, MASSACHUSETTS 01532		
PREPARED BY: J.M. GRENIER ASSOCIATES INC. 118 TURNPIKE ROAD SUITE 200 SOUTHBOROUGH, MASSACHUSETTS 01772		
SCALE: AS SHOWN	DATE: FEBRUARY 5, 2024	
SHEET NO.: SHEET 8 OF 9		
PROJECT NO.: G-684		



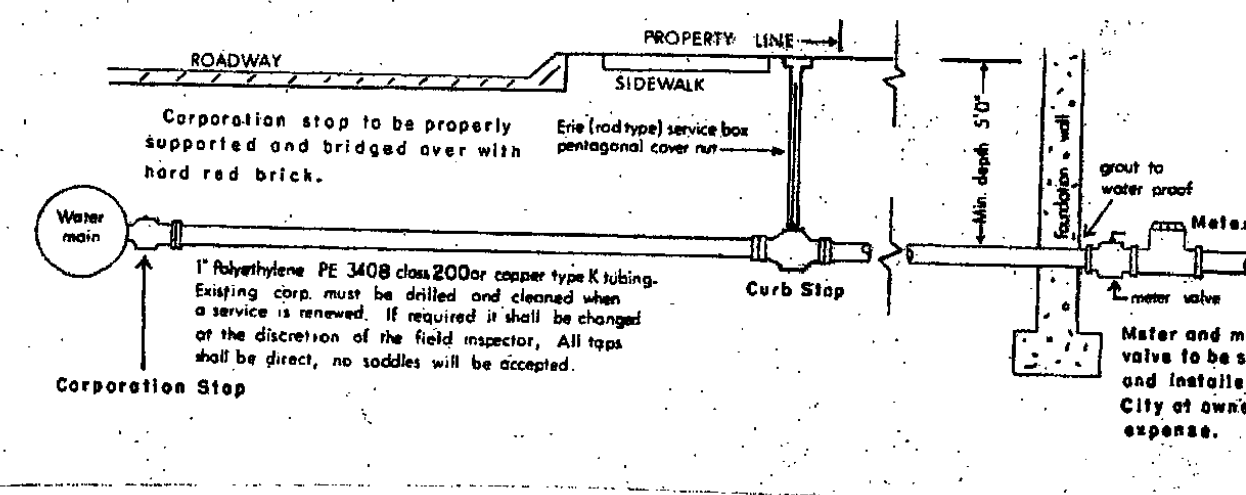
TYPICAL WATER TRENCH SECTION
NOT TO SCALE



SEWER TRENCH DETAIL
NOT TO SCALE

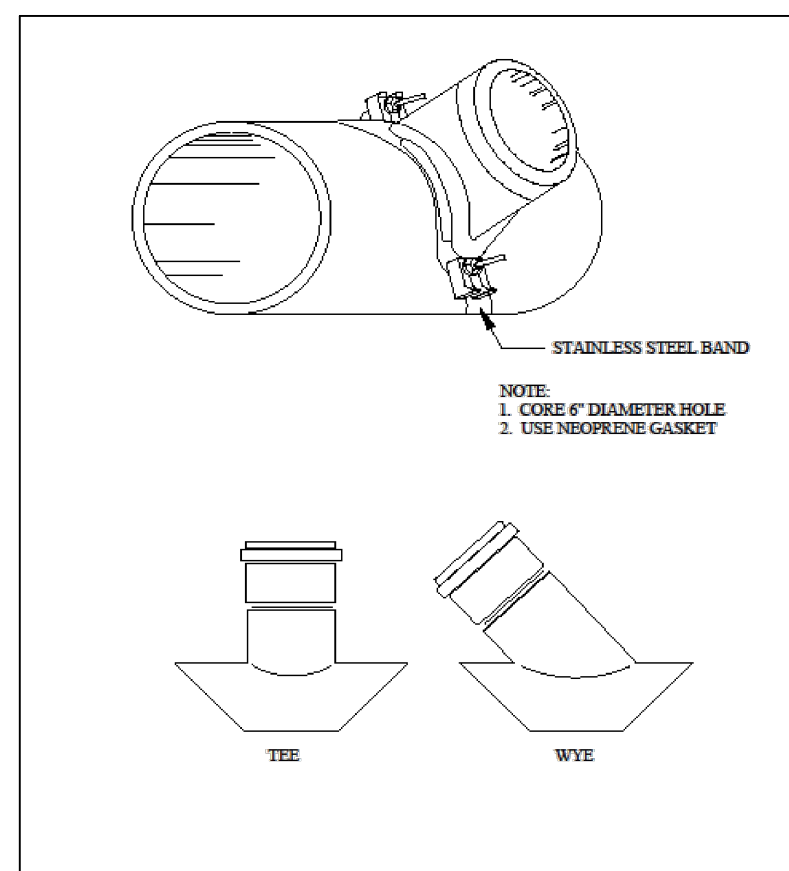


SIGN POST DETAIL
NTS

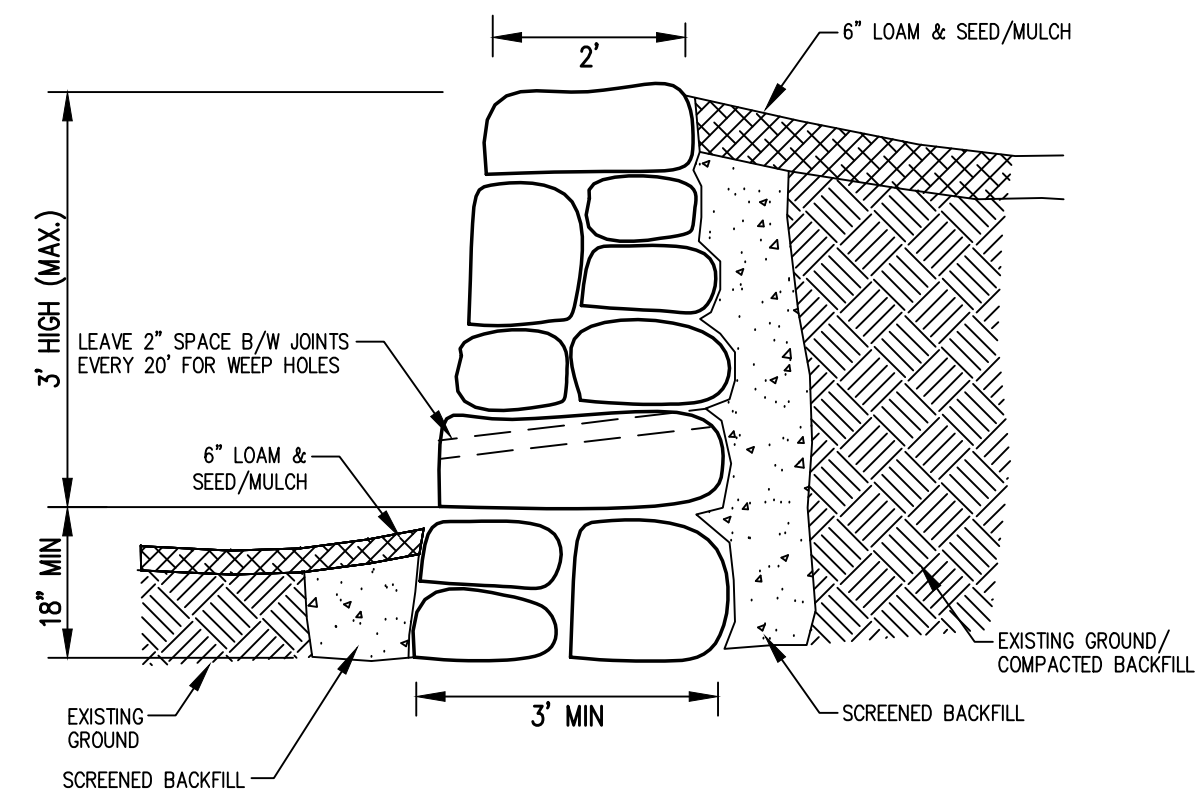


GENERAL NOTES

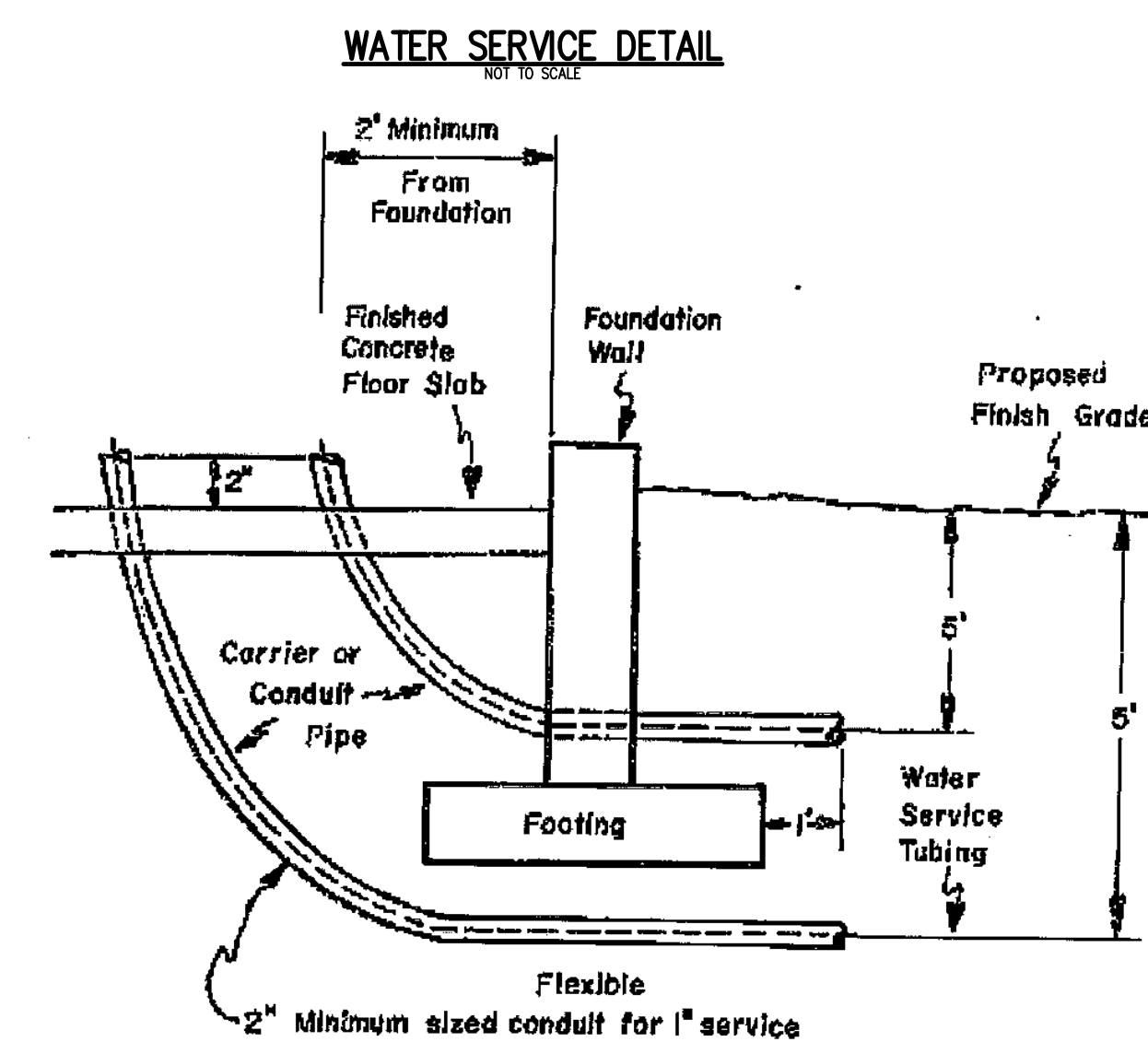
1. Fittings and valves shall be manufactured by Ford or approved equal. Corporations: Ford FB1000 c.c. thread by compression; Curb Stops: B44 series full port (comp. x comp.)
2. All compression fittings used on polyethylene tubing shall be installed with solid, tubular stainless steel insert stiffeners.
3. Sand shall be used for backfilling from a point 6 inches below the pipe to a point 12 inches above the pipe. The remaining backfill shall have no stones greater than 3 inches in their largest dimension. Before any excavation is done and any pipe is installed, permits shall be obtained from the Management Service office at 20 East Worcester St. and the Water Engineering office at 18 East Worcester St. All paving shall conform to City of Worcester D.P.W. standards.
4. Services shall be flushed before activating to avoid meter clogging.
5. Main valves are to be operated by the City only. Costs incurred in shutting down mains due to negligence of the Contractor as determined by the Field Inspector shall be paid by the Contractor. The Contractor shall inform the owner and the City wire inspector of proper grounding requirements pursuant to M.G.L. Chapter 165, Section 1C.
6. On new services, in accordance with the Mass. Department of Environmental Protection Code 5 and its amendments, the water service shall be a minimum of 10 horizontal feet from the sewer service.
7. The Contractor shall furnish and completely install the service from corporation to the meter valves with new materials.
8. Upon installation of the service box, the Contractor shall operate the stop to the satisfaction of the Field Inspector prior to acceptance of the job.



SEWER TEE & WYE CONNECTION
NOT TO SCALE



TYPICAL DRY STONE RETAINING WALL
NOT TO SCALE



GENERAL NOTES

1. When any water service 1-inch or less in size is to be installed under a concrete floor slab, the pipe shall be installed inside an approved conduit. This conduit shall be used as a sleeve or carrier pipe to receive the provided water service tubing.
2. The size of the conduit shall be at least twice the diameter of the water service pipe being installed.
3. The conduit shall be installed as shown in drawing W-6D which is part of these specifications.

WATER SERVICE ENTRANCE
NOT TO SCALE

REV. NO.	DATE	REVISION

TITLE:
**DEFINITIVE SITE PLAN FOR
114 AUSTIN STREET
WORCESTER, MASSACHUSETTS 01609**

PREPARED FOR:
**POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532**

PROPERTY OWNER:
**POLAR VIEWS, LLC
89 WEST MAIN STREET UNIT 101
NORTHBOROUGH, MASSACHUSETTS 01532**

PREPARED BY:
**J.M. GRENIER ASSOCIATES INC.
118 TURNPIKE ROAD SUITE 200
SOUTHBOROUGH, MASSACHUSETTS 01772**

TELE NO.: (508) 845-2500 jmgrenier@townisp.com

SCALE:	DATE:
AS SHOWN	FEBRUARY 5, 2024

DETAIL PLAN 2/2

SHEET NO.:	PROJECT NO.:
SHEET 9 OF 9	G-684

